

Sonoma County THE SPRINGS SPECIFIC PLAN

Draft - May 2022



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I. OVERVIEW

The Springs is a friendly, multi-cultural community with a rich history and an engaged citizenry. Located in central Sonoma Valley immediately north of the City of Sonoma, the Springs includes portions of the unincorporated areas of Agua Caliente, Fetters Hot Springs, Boyes Hot Springs. Covering approximately 178 acres, the Springs Specific Plan area is bounded by Agua Caliente Road at the north and Verano Avenue at the south and is bisected by the Highway 12 commercial corridor, which provides connectivity to communities and attractions throughout the region.

Plan Contents

The Specific Plan includes six chapters:

1. Introduction
2. Land Use
3. Circulation
4. Design Guidelines
5. Infrastructure
6. Implementation & Financing Plan

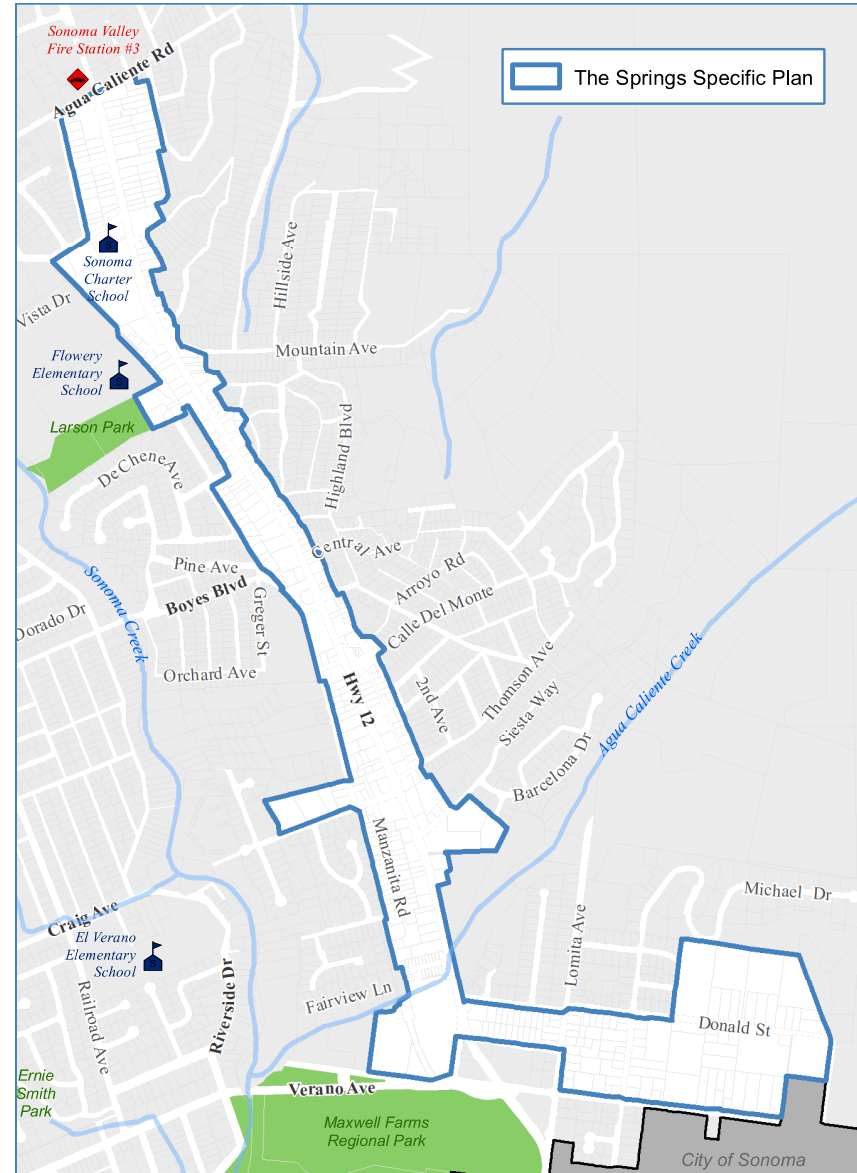


Figure 1: Springs Specific Plan

<p>Overview 1-1</p>	<p>Community Engagement 1-5</p>	<p>Guiding Principles 1-7</p>	<p>1-1</p>
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Purpose of the Plan

In 2015, Sonoma County embarked on an effort to create a community-driven Specific Plan for the Springs community. The Specific Plan is intended to foster a vibrant and attractive community with increased opportunities for housing and improved circulation for pedestrians, bicyclists, and transit. The Plan also designates a location for a community plaza and promote other public spaces while preserving the community’s character and scale.

The Springs Specific Plan is the primary planning document and reference guide for future development in the Springs. The Specific Plan is intended to be an expression of the community’s vision for the Springs and constitutes the policy and regulatory framework by which future development projects will be reviewed and public improvements will be implemented. The County will implement the Specific Plan by requiring development, infrastructure improvements, and other projects to be consistent with the policies and design guidelines of this plan.

Applying the Specific Plan

The Springs Specific Plan is intended for use by a broad range of persons, including:

- The Board of Supervisors, Planning Commission, Board of Zoning Adjustments, and Design Review Committee in decision-making activities;
- County staff in reviewing new projects;
- The development community in preparing development proposals; and
- Residents and businesses interested in understanding the community’s vision, goals, and priorities.

The Specific Plan applies to lands within the Plan area (see Figure 1), to the extent allowed by Federal and State law.

Interpreting the Specific Plan

Policies and guidelines that use the term “shall” or “must” are requirements; whereas, policies and guidelines that use terms such as “should” or “may” are recommendations and are intended to be implemented to the greatest extent feasible.

Statutory Requirements

Under California law, Sonoma County may adopt specific plans to establish policies, programs, regulations, and guidelines to implement the adopted general plan. All specific plans must comply with Sections 65450 through 65457 of the Government Code. Under these provisions, a specific plan must provide a framework for land use, infrastructure, development standards, and implementation. The Government Code also requires that a specific plan be consistent with the general plan. All new development, in turn, must be consistent with the specific plan.

CEQA Compliance

A Program EIR has been prepared for the Springs Specific Plan, consistent with the California Environmental Quality Act (CEQA). CEQA documentation for subsequent projects that are consistent with the Specific Plan is limited to analysis of effects that were not examined in the Program EIR, as described by the State CEQA Guidelines at Section 15168(c).

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Relationship to Other Documents

General Plan

The proposed policies and objectives of the Springs Specific Plan are consistent with the broad goals of the County's General Plan. The General Plan recognizes the Springs as a secondary commercial center in the Sonoma Valley area and encourages redevelopment by providing a flexible range of land uses within the Springs area. In addition, the General Plan encourages reducing the need for automobile use through improvements to the pedestrian, bicycle, and transit systems and through land development measures. In any case where there appears to be a conflict between the General Plan and any Specific or Area Plan, the more restrictive policy or standard shall apply.

Zoning

The Zoning Code was amended along with the adoption of this Specific Plan to ensure consistency between the two documents. The policies, development standards, and guidelines of this Specific Plan shall supersede those set forth in the Zoning Code.

Highway 12 Design Guidelines

The Design Guidelines chapter of this plan replaces the *Highway 12 Design Guidelines* (November 1994) which previously applied to the Plan area. The *Highway 12 Design Guidelines* were reviewed and select sections were incorporated into this Specific Plan, as appropriate.

II. COMMUNITY ENGAGEMENT

The Springs Specific Plan was developed using a robust and intensive program of community and stakeholder engagement activities, as described below.

Community Workshops

Five community workshops were held at key points in the planning process to facilitate public input in the development of the Specific Plan. The workshops focused on a variety of topics and sought to actively engage the community in a meaningful way through a combination of educational presentations and facilitated small group exercises.

Workshops 1 through 3 introduced the Specific Plan process to the community and were designed to elicit information from the community regarding the vision and goals for the Springs. Each workshop focused on a specific topic: Workshop 1 – Community Vision, Workshop 2 – Land Use and Transportation, and Workshop 3 – Community Design. Workshop attendees participated in a range of activities and exercises designed to gather information on community priorities, values, and focus areas for the Springs Specific Plan. Workshop participants also provided input on land use and circulation used to develop the land use and circulation alternatives presented in the *Land Use and Circulation Alternatives Report*. A summary of input from Workshops 1 through 3 is provided in the *Issues and Opportunities Summary*.



Participants in the Community Workshops provided input through small group mapping and visioning exercises, individual comments on comment boards, and voting on priorities and preferences.

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Workshop 4 presented the community with the three land use and circulation alternatives developed for the Specific Plan and described the characteristics of each alternative. Participants in Workshop 4 were provided an opportunity to vote for their preferred alternative and were also asked to complete a survey to identify which alternative best achieved specific goals for the Springs.

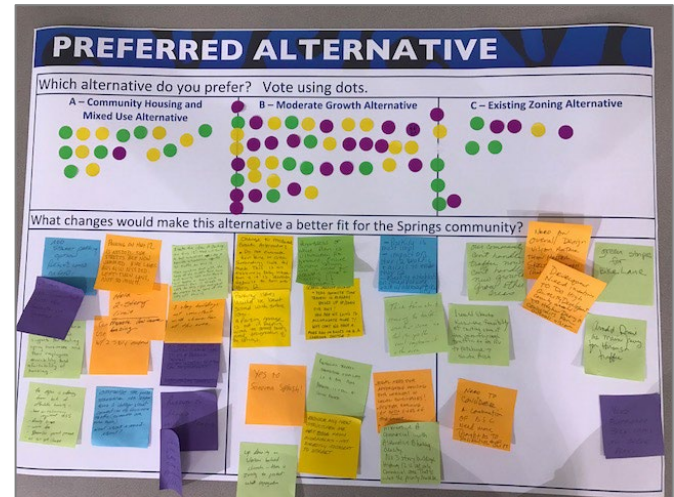
Workshop 5 introduced the community to the Draft Specific Plan and provided an opportunity for public comment.

Community Advisory Team

The Community Advisory Team (CAT), composed of residents and representatives of stakeholder organizations, was formed to represent the community and provide input to County staff and the Springs Specific Plan consultant team throughout the development of the Specific Plan. The CAT articulated views of residents and stakeholders in identifying the concerns and goals of the community, regularly reached out to the community while the Specific Plan was being developed in order to solicit input on priorities and issues of concern, encouraged participation in the community workshops, selected the preferred land use and circulation alternative, and commented on the draft Specific Plan. Their contribution to the process was significant and creation of this Specific Plan could not have been possible without their assistance.

Technical Advisory Committee

The Technical Advisory Committee (TAC) was assembled to provide guidance and feedback throughout the Specific Plan process to supplement feedback and direction provided by the CAT. The TAC included department staff from the County, Sonoma Transit, and other organizations and agencies.



Community Workshop 4 participants voted on their preferred land use and circulation alternative. The most votes were received for Alternative B, the Moderate Growth Alternative.

III. GUIDING PRINCIPLES

Key concepts pertaining to land use, community character, circulation, and parking, were identified during the Springs planning process. These concepts formed the basis for the "Guiding Principles" described below. The land use and zoning regulations, circulation requirements, and design guidelines developed for the Specific Plan were derived from these Guiding Principles.

1. ***Recognize and Promote the Springs Commercial Corridor as a mixed-use “Downtown” Serving the Larger Springs Community.*** The Springs Specific Plan encompasses the primary commercial district that serves as the “downtown” area of the larger Springs community. New commercial development along the Highway 12 corridor will increase the variety of retail shops and neighborhood services. New mixed-use development will help meet the housing needs of the community while providing pedestrian-oriented retail and restaurants. Wider sidewalks enhanced with pedestrian- and bike-friendly features will make it easier and more pleasant for residents to access local stores and services.
2. ***Develop a Centrally-Located Community Plaza.*** Provide a central gathering place where farmers markets, concerts, and other community events can take place to enhance the vitality of the Springs area. The Community Plaza should be designed to reflect the multi-cultural character of the community.
3. ***Celebrate the Unique, Multi-cultural Identity of the Springs.*** Recognize that the Springs is a diverse, multi-cultural community with significant historic resources and character. Ensure that new development respects the area’s treasured past.
4. ***Increase Affordable, Workforce, and Mixed Use Housing.*** Create new infill opportunities for higher density housing, while also expanding the variety of housing choices on vacant parcels in the Plan area.
5. ***Improve the Pedestrian, Bicycle, and Transit Network.*** Provide bicycle, pedestrian, and transit facilities throughout the Springs that are safe, well-lit, shaded, comfortable, well-connected, and accessible. This improved multimodal network will provide greater incentive for people to choose non-vehicular travel for their daily trips to reduce Vehicle Miles Traveled and support local climate goals. The Springs mobility network should recognize that non-vehicular travel is the primary travel mode for some residents.

6. ***Ensure an Adequate Parking Supply.*** Provide parking garages and/or surface parking lots adjacent to Highway 12, particularly in areas where there are existing parking shortages and near the areas planned for the community plaza and new community-serving commercial uses.
7. ***Address Community Safety.*** Create a safe environment for residents and employees by providing attractive, well-lit, and well-maintained public and community facilities that encourage regular use.
8. ***Create and Connect to More Parks and Open Space.*** Create new public and semi-public spaces, such as plazas, pocket parks, parklets, and green space, to create a desirable system of parks and community gathering areas.
9. ***Regional Planning.*** Assist the County in meeting its Regional Housing Needs Allocation by designating and zoning sites for higher densities and maintain consistency with the Priority Development Area designation by the Association of Bay Area Governments.

I. OVERVIEW

The Springs Specific Plan places a strong emphasis on increased housing opportunities, economic growth, and improved bicycle and pedestrian connectivity throughout the Plan area. Mixed-use, commercial, and medium to high density residential development will be accommodated along the Highway 12 corridor (see Figures 2 and 3). The variety of housing types included in the Plan accommodates a range of affordability levels. The Specific Plan also promotes new community-serving retail, restaurants, and services.

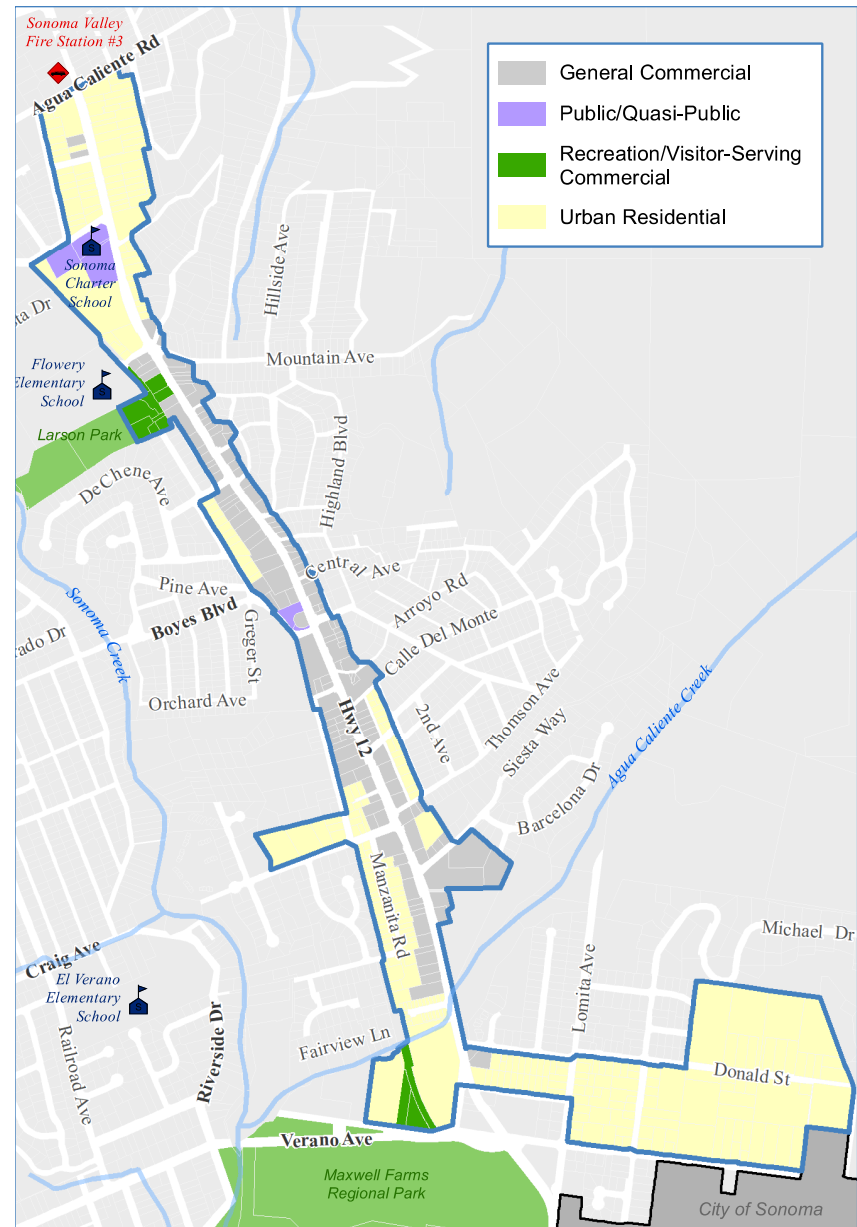


Figure 2: Land Use Map

<p>Overview 2-1</p>	<p>Zoning 2-4</p>	<p>Development Capacity 2-11</p>	<p>Goals & Policies 2-12</p>	<p>2-1</p>
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Mixed Use Corridor & Central Activity Hub



Mixed use example of retail and commercial uses on the ground floor and residential uses on the upper floors.

Mixed-Use Corridor. The Springs Specific Plan envisions a revitalized mixed-use corridor extending from Depot Road to Thomson Avenue, where residential and neighborhood commercial are concentrated around a central activity hub. This hub area would be strategically designed to provide pedestrian connectivity to a public plaza that would serve as the central community gathering area for the Springs.

Plaza. The plaza will accommodate various community activities, such as festivals, cultural events, concerts, and farmers markets. The plaza would provide seating, shade structures, and greenery for the community to enjoy. Neighborhood-oriented commercial uses would surround the plaza, providing opportunities for ground-floor restaurants and shops with residential units above.

Parking. Parking lots would be located along or just off the Highway 12 corridor to accommodate a portion of the parking demand generated by the community plaza, commercial, and mixed-use development.

Increased Housing Opportunities



Conceptual Plaza sketch.

The Springs Specific Plan includes a range of residential densities, providing opportunities for single family, multifamily, mixed use, and work/live units. Medium density residential land use designations are concentrated in the northern and southern portions of the planning area. Higher densities are provided north of Rancho Drive, on West Thompson Avenue, and in the Donald Street/Verano Avenue neighborhood.

Range of Local-Serving Uses

Local-serving Uses. The Specific Plan facilitates a variety of new local-serving commercial, office, and recreational uses. The land use map, supporting policies, and design guidelines are intended to encourage businesses oriented to the needs of the local community, rather than businesses with a regional focus.

Recreational Uses. Lands designated for Recreation and Visitor Serving Commercial are located in the southern portion of the Specific Plan, just north of Maxwell Park, and in the northwest, east of Larson Park. Although the zoning allows a variety of visitor serving uses, these areas are specifically intended to accommodate recreational uses that serve the community, such as parks, aquatic centers, sports fields, etc.



Local serving uses: restaurant.



Local serving uses: offices and retail.



Local-serving recreational use: Larson Park.

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II. ZONING

Zoning Districts. The Springs Specific Plan’s zoning districts are listed in Table 1. This table also includes a summary of permitted uses and standards for each zone. The Sonoma County Zoning Code should be consulted for a detailed list of allowed uses and specific development standards for each particular zoning district. However, the uses and standards of the Specific Plan would take precedence in the event that there is a conflict between the County’s Zoning Code and the Springs Specific Plan.

Zoning Maps. The zoning map for the Springs Specific Plan graphically shows all zoning district boundaries. This map may be consulted to determine the zoning for a particular parcel within the Specific Plan area (see Figure 3). Sonoma County’s official zoning database also provides the zoning for each parcel and is available on the Permit Sonoma website.

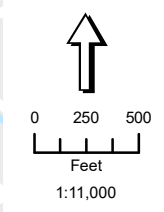
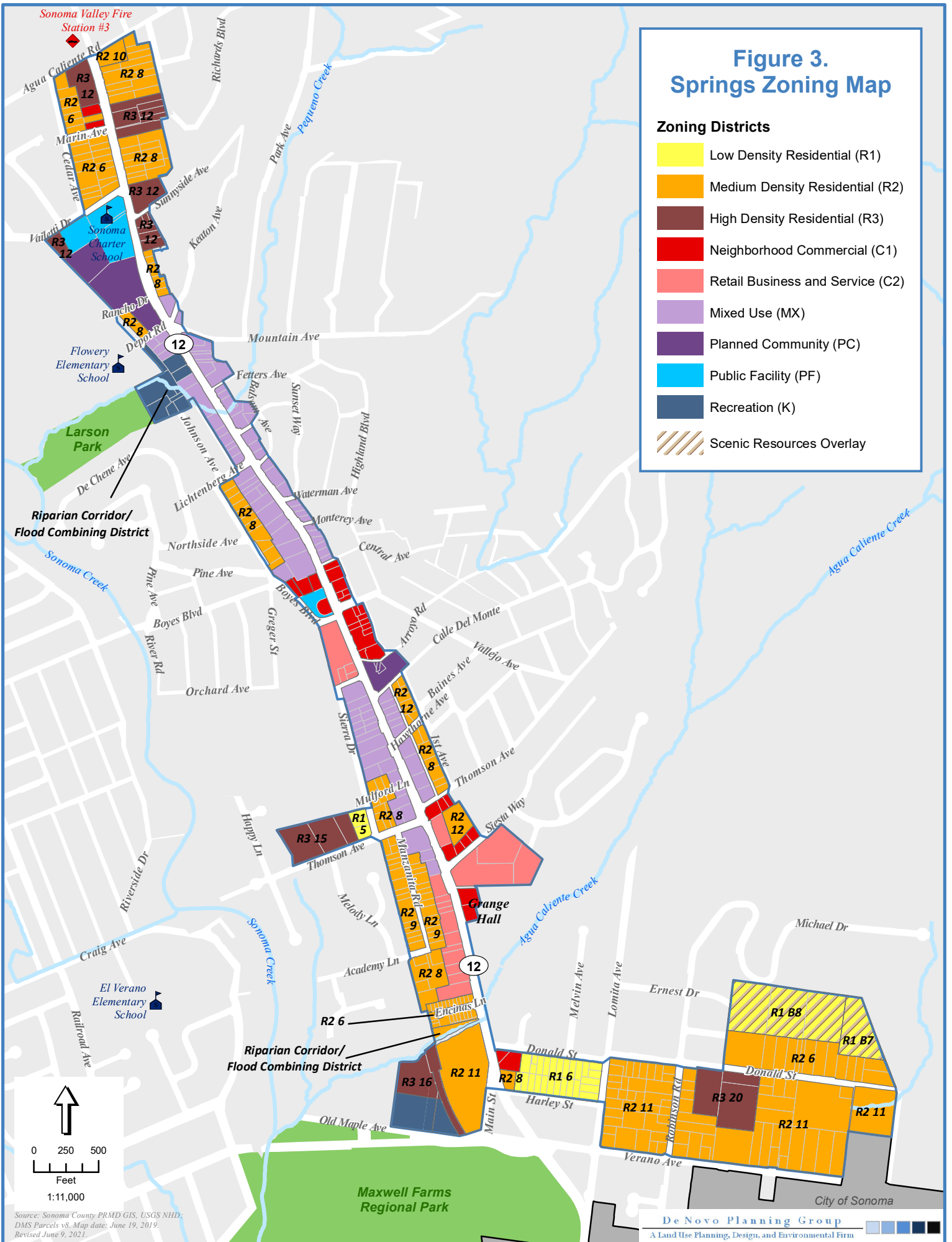
New Mixed-Use Zone. A new Commercial Mixed-Use (CM) zone is applied to the majority of parcels along the Highway 12 corridor, from about Keaton Avenue to Siesta Way. The Commercial Mixed-Use zone is intended to encourage mixed-use development and neighborhood-oriented commercial uses in the central commercial area of the Springs.

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Figure 3. Springs Zoning Map

Zoning Districts

- Low Density Residential (R1)
- Medium Density Residential (R2)
- High Density Residential (R3)
- Neighborhood Commercial (C1)
- Retail Business and Service (C2)
- Mixed Use (MX)
- Planned Community (PC)
- Public Facility (PF)
- Recreation (K)
- Scenic Resources Overlay



Source: Sonoma County PRMD GIS, USGS NHD, DMS Parcels v8. Map date, June 19, 2019. Revised June 9, 2021.

Table 1: Zoning Districts, Total Acres, Allowed Uses, and Standards Summary

Zoning District	Acres	Permitted Uses ¹	Standards
Low Density Residential (R1)	15.21	<ul style="list-style-type: none"> ▪ Single family ▪ Accessory dwelling unit ▪ Junior accessory dwelling unit ▪ Cottage housing 	Density: 4 to 6 units per acre <i>R1 5: 5 units per acre</i> <i>R1 6: 6 units per acre</i> <i>R1 B8: No further subdivision</i> Minimum lot size: 6,000 square feet Main building height: 35 feet
Medium Density Residential (R2)	68.85	<ul style="list-style-type: none"> ▪ Single family attached & detached ▪ Accessory dwelling unit ▪ Junior accessory dwelling unit ▪ Duplex ▪ Triplex ▪ Fourplex ▪ Multifamily ▪ Cottage Housing ▪ Single Room Occupancy 	Density: 6 to 12 units per acre <i>R2 6: 6 units per acre</i> <i>R2 8: 8 units per acre</i> <i>R2 9: 9 units per acre</i> <i>R2 10: 10 units per acre</i> <i>R2 11: 11 units per acre</i> <i>R2 12: 12 units per acre</i> Minimum lot size: 4,000 square feet Main building height: 35 feet
High Density Residential (R3)	17.39	<ul style="list-style-type: none"> ▪ Single family attached ▪ Accessory dwelling unit ▪ Junior accessory dwelling unit ▪ Micro apartments ▪ Duplex ▪ Triplex ▪ Fourplex ▪ Multifamily ▪ Single Room Occupancy 	Density: 12 to 20 units per acre <i>R3 12: 12 units per acre</i> <i>R3 16: 16 units per acre</i> <i>R3 20: 20 units per acre</i> Minimum lot size: 4,500 square feet Main building height: 35 feet, except maximum 40 feet for three stories

¹ Planning Permits may be required.

² Floor area ratio is based on the lot coverage multiplied by the number of building stories allowed as a permitted use; 35 ft building heights are assumed to allow two stories and 40 foot or greater building heights are assumed to allow three stories.

Zoning District	Acres	Permitted Uses ¹	Standards
Planned Community (PC)	4.94	The PC district allows for a range of uses that are consistent with the General Plan land use designation for the parcel.	<p><u>Residential Density:</u> As allowed by the General Plan, subject to any zoning restrictions</p> <p><u>Non-Residential</u></p> <p>Maximum floor-area-ratio²: 1.0</p> <p>Lot coverage: 50%</p> <p>Building height: 35 feet</p>
Neighborhood Commercial (C1)	6.50	<ul style="list-style-type: none"> ▪ Neighborhood retail ▪ Restaurants ▪ Neighborhood and community services ▪ Offices ▪ Mixed Use ▪ Work/Live units <p><u>Prohibited Uses</u></p> <ul style="list-style-type: none"> ▪ Adult-oriented business ▪ Cannabis-related uses ▪ Convenience store sale of alcoholic beverages ▪ Drive-in or drive-through uses ▪ Mobile Food Trucks ▪ Industrial uses ▪ Tobacco/Smoking related sales or use (as a Primary Use) ▪ Vehicle Oriented Uses: auto sales, rental, service, repair, car wash, fueling, tire, and part sales, etc. 	<p>Maximum floor-area-ratio²: 1.0</p> <p>Lot coverage: 65%</p> <p>Building height: 35 feet</p>

¹ Planning Permits may be required.

² Floor area ratio is based on the lot coverage multiplied by the number of building stories allowed as a permitted use; 35 ft building heights are assumed to allow two stories and 40 foot or greater building heights are assumed to allow three stories.

Zoning District	Acres	Permitted Uses ¹	Standards
Retail Business and Service (C2)	10.49	Community Retail Auto repair and services Restaurants Financial institutions Theaters	Maximum floor-area-ratio: 1.0 Lot coverage: 50% Building height: 35 feet
Mixed-Use Community (CM)	22.31	<ul style="list-style-type: none"> ▪ <u>Ground Floor of Mixed-Use or Single-Story Commercial</u> <ul style="list-style-type: none"> • Neighborhood-serving retail: Grocery stores, drug stores, book stores, gift shops, floral shops, art supplies, candy and ice cream shops, etc. • Community-oriented services: Hair salons, barber shops, child day care, etc. • Restaurants & retail food: Restaurants, coffee & tea shops, bakeries, candy and ice cream shops, sale of other foods • Offices existing as of date of Plan adoption • Public Facilities ▪ <u>Upper floor(s)</u> Multifamily residential, office 	Maximum floor-area-ratio (mixed-use): 2.0 Maximum floor-area-ratio (other): 1.0 Lot coverage: 65% Building height: 35 feet

¹ Planning Permits may be required.

² Floor area ratio is based on the lot coverage multiplied by the number of building stories allowed as a permitted use; 35 ft building heights are assumed to allow two stories and 40 foot or greater building heights are assumed to allow three stories.

Zoning District	Acres	Permitted Uses ¹	Standards
		<p>Other Uses</p> <ul style="list-style-type: none"> ▪ Parking (stand alone) ▪ Community serving uses: Library, schools, museums, clinics, post office, etc. ▪ Work/live units <p>Prohibited Uses Same prohibited uses as C1 district.</p>	
Recreation and Visitor Serving Commercial (K)	5.12	<ul style="list-style-type: none"> ▪ Public parks ▪ Aquatic centers ▪ Sport fields ▪ Retail as part of recreational use 	Maximum floor-area-ratio: 1.0 Lot coverage: 50% Building height: 35 feet
Public Facilities (CF)	4.24	County- and city-owned facilities Special district facilities for utilities Schools	Maximum floor-area-ratio: 0.8 Lot coverage: 40% Building height: 35 feet

¹ Planning Permits may be required.

² Floor area ratio is based on the lot coverage multiplied by the number of building stories allowed as a permitted use; 35 ft building heights are assumed to allow two stories and 40 foot or greater building heights are assumed to allow three stories.

Nonconforming Uses and Structures

Nonconforming Use. A nonconforming use is a use that was legal at the time it was established, but which is no longer allowed because of subsequent changes to zoning or adoption of this Specific Plan.

A legal non-conforming use may continue to operate so long as the use does not increase in area or intensity. Once a legal non-conforming use ceases for one year, the use cannot be re-established. Subsequent uses must comply with current zoning.

Nonconforming Structure. A structure that was constructed legally, but which no longer conforms to current density or development standards, such as setbacks and lot coverage, due to subsequent changes to zoning or adoption of this Specific Plan.

Remodeling, and ordinary maintenance and repairs, may be made to a legal nonconforming structure as allowed by the Sonoma County Zoning Ordinance.

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III. DEVELOPMENT CAPACITY

The development projections shown below (Table 2) are an estimate of the new residential units and commercial, office, and recreational square footage that could develop under the Specific Plan’s zoning (Figure 3). These projections are based on a study of opportunity sites that have the potential for new development or intensification of existing uses.

Actual future development in the Springs could vary significantly from these projections, depending on future market conditions, property owner preferences, site-specific constraints, density bonuses, or other factors.



Fetters Apartments construction photo.

Table 2: New Development Projections

Type of Development	Residential Units	Non-Residential Development
Single Family	88	-
Multifamily	461	-
Mixed Use or Live Work	127	-
Commercial	-	168,029 square feet
Office	-	82,226 square feet
Recreation	-	26,648 square feet; 120 hotel rooms
TOTAL	706	276,903 square feet 120 hotel rooms

IV. LAND USE GOALS AND POLICIES

Note: Policies associated with circulation, including pedestrian, bicycle, and transit facilities, and parking, are located in the Circulation chapter.

Goal SLU-1: Promote a Vibrant Mix of Residential, Local-Serving Commercial, and Recreational Uses that Celebrate the History of the Springs.

Goal SLU-2: Create a Pedestrian-Scale, Community-Oriented Built Environment Organized around a Community Plaza.

Goal SLU-3: Establish the Springs as a Great Place to Live and Work, as well as a Destination for Shopping and Entertainment.



Community-oriented development with pedestrian-scale amenities and features to encourage social gathering.

Policy SLU-1a: Support development that is consistent with the overall principles, policies, land use concept, and design guidelines included in this Specific Plan.

Policy SLU-1b: Ensure that new and redeveloped buildings are compatible with the traditional architectural character of the Springs in terms of scale, height, and design. Development projects must also integrate well with surrounding development.

Policy SLU-1c: Ensure that all development projects be designed to contribute to a visually rich, pedestrian-friendly streetscape by providing architectural interest at the street level and pedestrian-oriented amenities, such as awnings, planters, benches, etc.

Policy SLU-1d: Create a central gathering place for the greater Springs community, surrounded by small-scale restaurants, shops, and other pedestrian-oriented uses.

Policy SLU-1e: Ensure that land uses complement one another.

Policy SLU-1f: Locate a rich mix of transit-supportive land uses within a comfortable walking distance of transit stops.

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Policy SLU-1g: Support the establishment, preservation, and expansion of community-serving uses such as parks, community centers, youth facilities, cultural and art centers, and a library.

Policy SLU-1h: Support the preservation and expansion of retail and service uses that serve the daily needs of local residents.

Policy SLU-1i: Require development to incorporate sustainability measures, such as setbacks from creeks and sensitive habitats, use of native or drought tolerant plants, permeable concrete or pavers, and minimal night lighting in the vicinity of creeks and habitat corridors, whenever appropriate.



Transit-supportive land uses include medium and high density housing, shopping, and restaurants. Note: Revise picture to show second story housing/balconies and bus stop.

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Policy SLU-1k: Explore and support the establishment of an Arts District in the Springs.

Policy SLU-1l: Encourage a variety of art, music, drama, and festivals at the new community plaza and at other locations throughout the Springs.

Policy SLU 1-m: Require the adaptive reuse of historic and architecturally significant buildings.



La Luz provides educational, financial, health, and other supportive services, with a focus on providing a safe place for immigrants to receive necessary services.



Teen Services Sonoma operates a local teen center providing services to the Spring's teen population.

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Goal SLU-4: Provide a Range of Housing Types and Densities for Individuals and Families of All Income Levels and Encourage Affordable and Workforce Housing.

Policy SLU-2a: Continue to implement programs and policies to facilitate development of affordable and workforce housing within the Springs.

- State Density Bonus Program
- County Supplemental Density Bonus Program
- Mixed Use Density Bonus Program
- Rental 100% Density Bonus Program
- Ownership 100% Density Bonus Program

Policy SLU-2b: Prioritize the Springs when funding for affordable housing becomes available, such as the County’s Affordable Housing fund, HOME, CDBG, and other funding sources.

Policy SLU-2c: Develop an incentive program for new commercial, office, and private recreation development projects to encourage workforce housing, work/live units, or mixed use residential units.

Policy SLU-2d: Consider establishing a Workforce Housing Opportunity Zone or a Housing Sustainability District within the Springs as provided by state legislation.



Fetters Apartments provides housing affordable to the local workforce and seniors. The MidPen development includes a community garden, shared playground with Sonoma Charter School, and space for community supportive services. (Photo source: MidPen Housing)

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Policy SLU-2e: Units that are smaller than 500 square feet shall be counted as 0.5 dwelling unit for the purpose of calculating maximum density for a parcel.

Policy SLU-2f: Require that new single-family residential developments include small units, accessory units, or corner lot duplexes to increase the supply of workforce housing.

Policy SLU-2g: Promote the construction of junior accessory dwelling units which create an invisible increase in density.

Policy SLU-2h: Allow work/live units in all commercial and mixed-use zones in the Springs planning area, subject to County standards and requirements for work/live units.



Duplex through fourplex units and mixed use, multifamily projects are appropriate for infill opportunities throughout the Springs, providing variety in the housing stock and increasing workforce and affordable housing options.



A work/live use is a business conducted from a residence or accessory structure by the resident and up to two employees.

2-16	Overview 2-1	Zoning 2-4	Development Capacity 2-11	Goals & Policies 2-12
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Goal SLU-5: Provide a Variety of Community Gathering Places throughout the Springs.

Policy SLU-3a: Encourage and facilitate a variety of public and semi-public outdoor spaces for dining, relaxing and social interaction, such as plazas, paseos, pocket parks, parklets, and courtyards throughout the Springs and at the locations shown in Figure 4.

Policy SLU-3b: Facilitate the development of a community plaza in a central location. The plaza should be sufficiently large to accommodate a range of community-oriented uses, such as farmer’s markets, festivals, cultural events, and concerts.

Policy SLU-3c: Promote the value of streets and public rights-of-ways as public places and encourage streetscape improvements that incorporate public seating and other community gathering opportunities whenever feasible.



*Conceptual plaza design.
Design Concept Image:
RossDrulisCusenbery
Architecture, Inc.*

Policy SLU-3d: Allow the establishment of parklets as an extension of the sidewalk, in lieu of on-street parking in select locations along Highway 12 and adjoining streets, for use as curbside dining, a resting place for shoppers, and social interaction.

Policy SLU-3e: Require that community open space include shade, seating, greenery, and other amenities that encourage public use and make the Springs an inviting, walkable community.

Policy SLU-3f: Allow locally-oriented recreational uses in the Recreation and Visitor Serving Commercial zone, rather than visitor or regional-serving uses.

Policy SLU 3g: Maintenance of pocket parks and parklets should be provided by the associated development project and should have long-term funding through a Homeowners Association or similar mechanism.



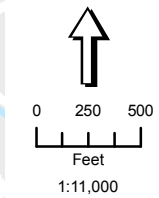
Parklets are typically small open spaces that extend sidewalks to provide public gathering spaces along streets. Parklets can be temporary or permanent installations in place of parallel parking spaces to provide uniquely designed publicly accessible open spaces.

2-18	Overview 2-1	Zoning 2-4	Development Capacity 2-11	Goals & Policies 2-12
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- Streetscape and Pedestrian Improvements
- County Park
- Springs Plaza
- Community Place
- New Pocket Park/Community Place
- Pocket Park

Figure 4.
Parks, Community Gathering Areas, and Open Space



Source: Sonoma County PRMD GIS, USGS NHD, Parcels v7.
Map date: June 26, 2017, Revised February 6, 2019.

Policy SLU-3h: Require the inclusion of community open space, such as outdoor seating areas, as part of new development projects.

Policy SLU-3i: Explore funding opportunities to finance the development of community open space at key locations along the Highway 12 corridor.

Policy SLU-3j: Encourage developments to restore adjacent creeks and feature them in the project design.

Policy SLU-3k: Require that plazas, parklets, outdoor seating areas, and other community spaces, are well-designed and constructed of high-quality, durable materials to ensure that these spaces remain attractive and functional for years to come.

Policy SLU-3l: Allow all types of public open space, including parks, parklets, pocket parks, and plazas, in all zoning districts in the Springs planning area.

Larson Park provides visitors and opportunity to enjoy the area's open space and creeks.



A pocket park is a small outdoor space for people to gather, relax, and enjoy the outdoors. A pocket park is usually only a few house lots in size or smaller, most often located in an urban area surrounded by development. (Image source: Centerline Architects)

2-20	Overview 2-1	Zoning 2-4	Development Capacity 2-11	Goals & Policies 2-12
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Goal SLU-6: Uphold and Celebrate the Residential Character of the Donald/Verano Neighborhood.

Policy SLU-6a: Enhance the neighborhood’s identity through development, features, and site and building design that are compatible in scale and in character with the existing neighborhood.

Policy SLU-6b: Facilitate infill development, redevelopment of underutilized land, adaptive reuse, and the restoration of existing buildings while requiring new development to complement the neighborhood context, densities, architectural styles, and overall pattern of existing development.

Policy SLU-6c: Create effective transitions between lower residential densities and mid- to upper-range densities, such as scaling new development so that portions of buildings that are adjacent existing residences are similar in height, form, and scale, providing compatible setbacks, or through increasing open space or landscaping.



A vineyard is one of the uses that characterizes the northern boundary of the residential neighborhood.



Residences in the neighborhood reflect varied types of architecture including bungalow, ranch, mid-century modern (pictured right), and modern (pictured left) styles.



<p>Overview 2-1</p>	<p>Zoning 2-4</p>	<p>Development Capacity 2-11</p>	<p>Goals & Policies 2-12</p>	<p>2-21</p>
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I. OVERVIEW

The Springs circulation plan promotes multimodal transportation by enhancing the design of Highway 12 to accommodate wider sidewalks, buffered bicycle lanes, and improved transit stops. New parking facilities are also planned for the Springs.

The Specific Plan includes policies that support additional investment in transit routes linking the Springs and downtown Sonoma in order to improve regional connectivity, reduce automobile reliance, and decrease parking demand throughout the Springs. The improvements to the bicycle, pedestrian, and transit facilities included in this Plan are intended to facilitate mobility within the Specific Plan area as well as improve access to nearby communities and regional facilities, such as medical centers, parks, open space, and other destinations. Approaches to funding the circulation improvements in the Springs are discussed in Chapter 6, Implementation. Circulation policies begin on page 3-20 of this chapter.



With Highway 12 serving as the primary roadway through the Springs, the Specific Plan area must address all modes of transportation, ensuring that pedestrians, bicyclists, and transit users are accommodated and provided appropriate facilities that do not conflict with vehicle traffic.

Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20	3-1
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II. PEDESTRIANS

Getting around the Springs on foot had been very challenging for residents until completion of the recent improvements to the Highway 12 corridor. The highway’s narrow shoulders were replaced with bike lanes, 5-1/2-foot wide sidewalks, and pedestrian street lights. The Specific Plan builds on these vital safety improvements by providing improved pedestrian connections, wider sidewalks, elimination of sidewalk gaps, new crosswalks, and pedestrian-scale amenities to encourage walking and make the Plan area a more pleasant and interesting place for people who choose to walk.

Improvements to sidewalks and crosswalks are described below and are summarized in Table 3. A map showing the locations of improvements to the pedestrian circulation system is also included (Figure 5).

Sidewalks. Sidewalks along the Highway 12 corridor are planned to be widened by four feet. The 9.5 foot sidewalks envisioned by the Specific Plan will occur incrementally as new development or significant redevelopment occurs along Highway 12. Wider sidewalks will accommodate amenities, such as benches, street trees, newspaper stands and bicycle racks. Sidewalk improvements would occur as part of new development or redevelopment projects.

The Plan includes a continuous sidewalk along the Highway 12 corridor. Gaps in the sidewalks will be eliminated and the Agua Caliente Creek bridge will be widened to accommodate a sidewalk. Sidewalk gaps along side streets would be filled as parcels develop or redevelop and when new street parking is constructed.

3-2	Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20
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Crosswalks. Pedestrian improvements along Highway 12 include the addition of 11 new crosswalks, which would reduce the average distance between marked crosswalks by more than half, from approximately 950 feet to 450 feet.

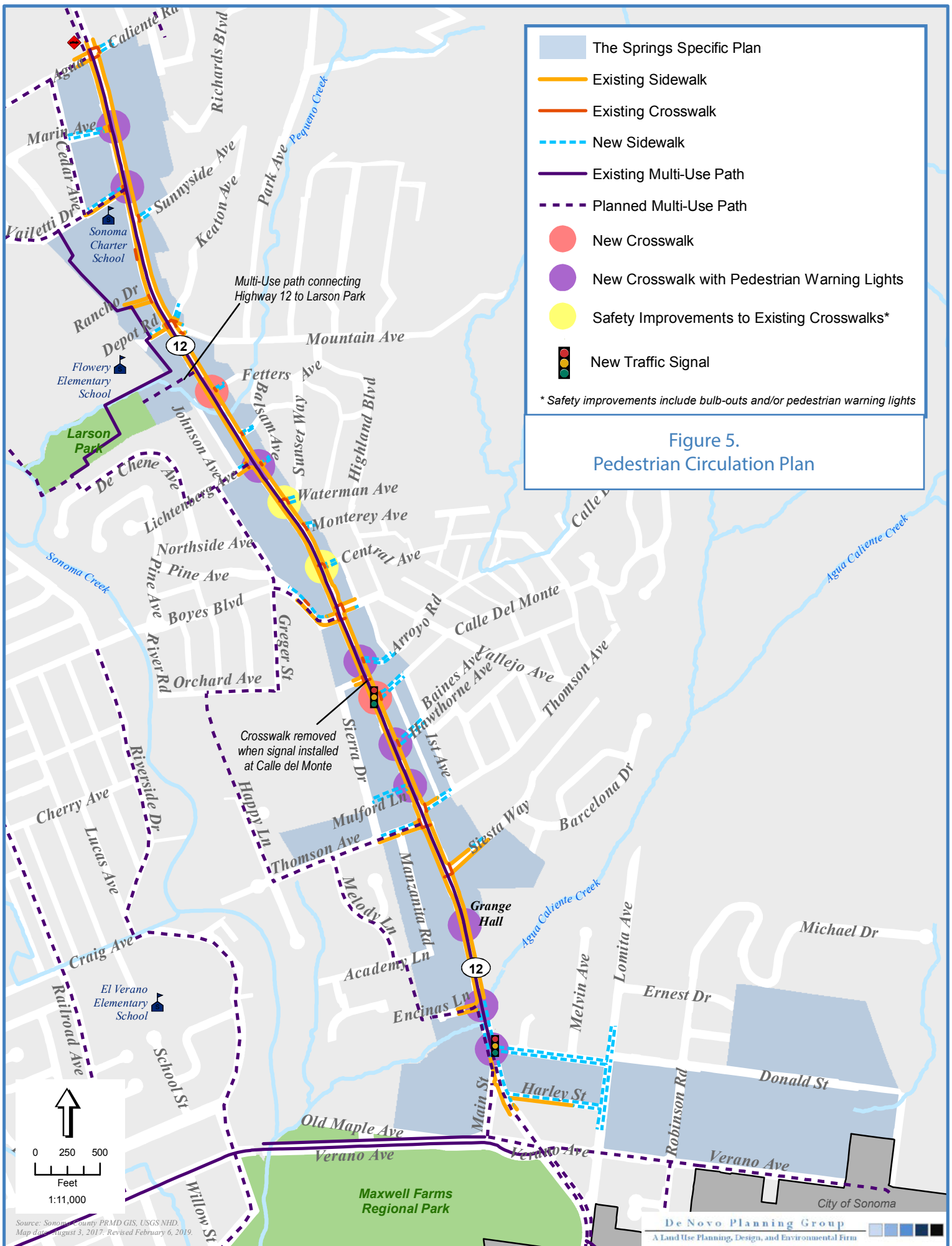
Many of the new crosswalks would include curb extensions (bulb-outs) which extend the sidewalk into the parking lane to narrow the roadway. Curb extensions enhance pedestrian safety by shortening crossing distances and increasing pedestrian visibility. Curb extensions are also widened in key locations and enhanced with special paving to create public spaces and landscaped areas.

Pedestrian warning lights, such as flashing beacons, would be provided where crosswalks are located near schools and transit stops. One mid-block pedestrian crossing with curb extensions and warning lights is provided near the Grange Hall, since there are no intersections between Siesta Way and Encinas Lane. Pedestrian refuge islands are to be incorporated into the crossings at Marin Avenue, Arroyo Road, and Hawthorne Avenue. The additional street crossing opportunities included in the Plan improve pedestrian mobility along the Highway 12 corridor and increase safety.



Clearly defined crosswalks and features such as bulb-outs increase pedestrian visibility to vehicles.

Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20	3-3
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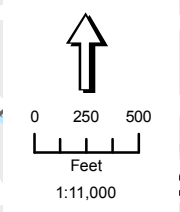


Legend:

- The Springs Specific Plan
- Existing Sidewalk
- Existing Crosswalk
- New Sidewalk
- Existing Multi-Use Path
- Planned Multi-Use Path
- New Crosswalk
- New Crosswalk with Pedestrian Warning Lights
- Safety Improvements to Existing Crosswalks*
- New Traffic Signal

* Safety improvements include bulb-outs and/or pedestrian warning lights

Figure 5.
Pedestrian Circulation Plan



Source: Sonoma County PRMD GIS, USGS NHD.
Map date: August 3, 2017. Revised February 6, 2019.

Table 3: Pedestrian Facility Improvements

Location	Improvement	Timing/Need
Highway 12		
Central Avenue	New warning lights at existing crosswalk	Install near-term. Existing commercial district core with frequent pedestrian activity
Fetters Avenue	New crosswalk (south side), bulbout (west side)	Install near-term. Adjacent to future pathway connection to Larson Park, serves planned mixed-use development, reduces distance between marked crosswalks from approximately 1,600 feet to 950 feet
Vailletti Drive	New crosswalk (north side), bulbouts, warning lights	Install near-term. Adjacent to school, 400' south of bus stops at Marin Ave, reduces distance between marked crosswalks from approximately 2,000 feet to 1,000 feet
Agua Caliente Drive	No change	-
Depot Road	No change	-
Lichtenberg Avenue	New crosswalk (north side), bulbouts, warning lights	Long-term. Serves planned mixed-use development, install as pedestrian crossing demand increases.
Waterman Avenue	New bulbouts at existing crosswalk	Long-term. Install when Highway 12 is modified to include on-street parking.
Boyes Boulevard	No change	-
Arroyo Road	New crosswalk (north side), bulbouts, median refuge, warning lights	Long-term. Serves core commercial area and future neighborhood retail/mixed-use development, install as pedestrian crossing demand increases.
Sierra Drive	Remove crosswalk upon signalization of Calle del Monte intersection	Long-term. Adjacent crosswalk at Calle del Monte is just over 100 feet to the south and will include pedestrian crossing phases; public notice will be required prior to removal of the existing crosswalk.
Calle del Monte	New crosswalk once intersection is signalized	Long-term. Install crosswalks and implement pedestrian phasing as part of intersection signalization.

Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20	3-5
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Hawthorne Avenue	New crosswalk (south side), bulbouts, median refuge, warning lights	Long-term. Near southbound bus stop, adjacent to existing businesses and on-street parking area, should occur when Highway 12 cross-section is modified on this segment to include on-street parking.
W Thomson Avenue	No change	-
Siesta Way	No change	-
Encinas Lane	New crosswalk, bulbouts, warning lights (post bridge widening)	Long-term. Install as part of Agua Caliente Creek bridge widening project.
Donald Street	New crosswalk (south side), bulbouts, warning lights (occurs after sidewalks completed on Highway 12 and Donald Street)	Long-term. Install upon signalization of the intersection and completion of new sidewalks across widened Agua Caliente Creek bridge.
Marin Avenue	New crosswalk (north side), bulbouts, median refuge, warning lights	Optional. Serves residential and neighborhood commercial uses, adjacent to bus stops on Highway 12, install as pedestrian crossing demand increases.
Mulford Lane	New crosswalk (north side), bulbouts, warning lights	Optional. Install as adjacent mixed-use development occurs and pedestrian crossing demand increases.
South of Grange Hall	New crosswalk, bulbouts, warning light (midblock)	Install as adjacent retail development occurs and pedestrian crossing demand increases
Sidewalks		
Highway 12	Complete sidewalk (Encinas Lane to Harley Street) Widen bridge over Agua Caliente Creek Widen sidewalks	Near-term. Complete sidewalks. Near-term. Widen bridge. Long-term. Widen sidewalks concurrent with development projects.
Side Streets	Add sidewalks and adjacent to new on-street parking	Long-term. Complete sidewalks concurrent with development projects.
Donald-Verano Area	Fill sidewalk gaps	Near-term.

III. BICYCLISTS

Highway 12 Bicycle Lanes. The Specific Plan improves bicycle safety in the Springs through the use of buffers and colored pavement along the main corridor. The existing 8-foot wide bike lanes on Highway 12 would be converted to 5-foot wide bike lanes with buffers of striped pavement to separate bicyclists from automobiles. Green colored bike lanes will be used in areas where bike and vehicle traffic interact, such as near intersections. The green pavement enhances bicycle safety by alerting drivers to the likely presence of bicyclists.

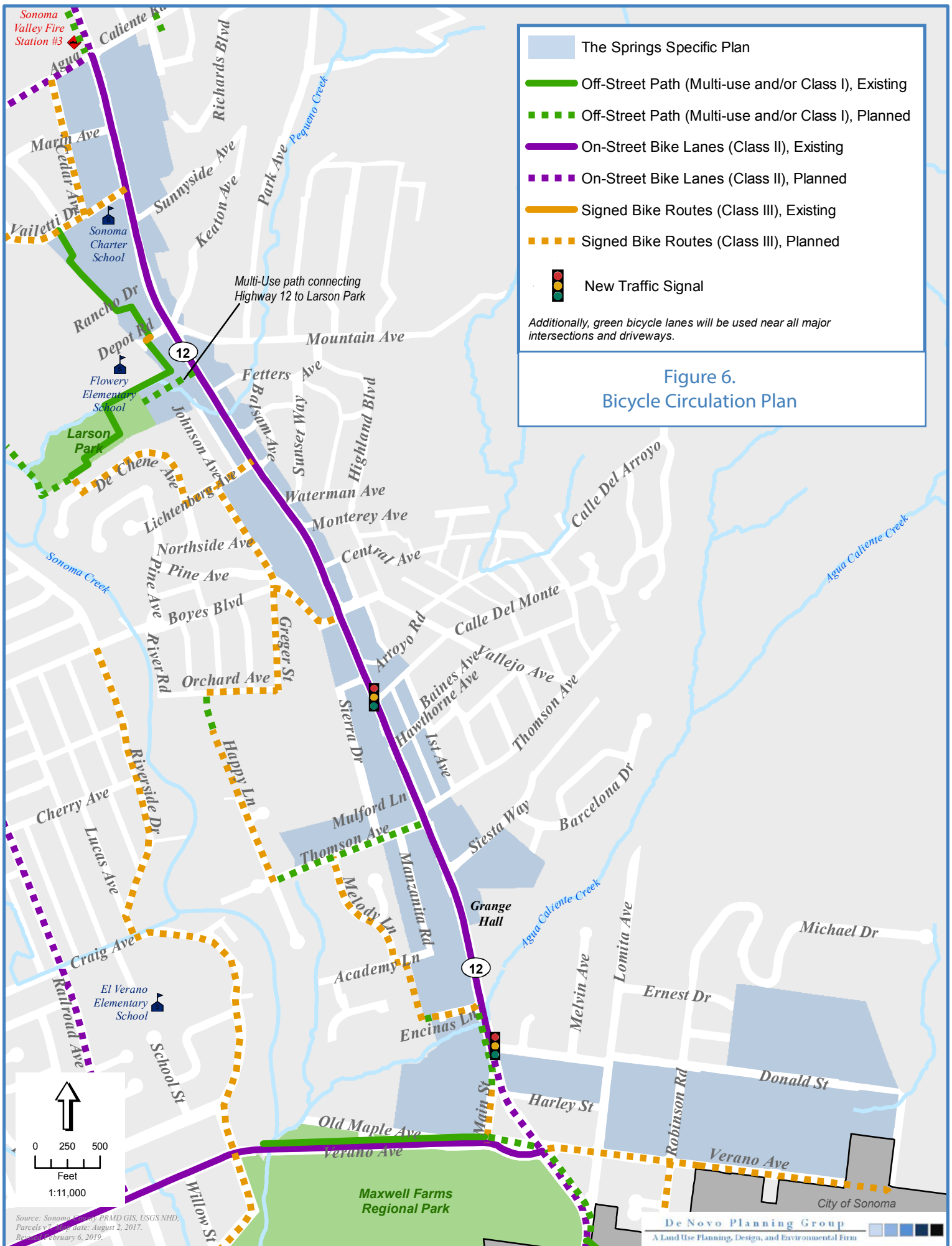
Central Sonoma Valley Bikeway. Bicycle safety will also be improved with the development of new multi-use paths and bike lanes as part of the Central Sonoma Valley Bikeway (see Figure 6). New multi-use paths in the planning area include a segment of West Thomson Avenue, between Happy Lane and Melody Drive. An off-street path is also planned west of Highway 12, between Encinas Lane and Main Street.

The Central Sonoma Valley Bikeway also includes new on-street bicycle routes identified with posted signs. These routes, when combined with recently-completed and proposed off-road bikeway segments, will result in a continuous north-south bike facility through the Springs that provides an alternative to biking along Highway 12.

Route Connections. The Specific Plan also includes key connections to existing and proposed bike routes to improve bicycle circulation in the area. Lichtenberg Avenue and a segment of Boyes Boulevard, between Highway 12 and Greger Street, are designated bicycle routes which will be identified with posted signs. Valetti Drive, between Highway 12 and Lake Street, create a new bike connection between the Central Sonoma Valley Bikeway and Highway 12. And a new multi-use path should be provided to connect Larson Park to Highway 12 at the approximate location shown on the bicycle circulation map (Figure 6). Finally, the Specific Plan extends the West Thomson Street off-street bike path to Highway 12.

Table 4 summarizes bicycle and multi-use facility improvements anticipated within the Specific Plan.

Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20	3-7
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The Springs Specific Plan
 Off-Street Path (Multi-use and/or Class I), Existing
 Off-Street Path (Multi-use and/or Class I), Planned
 On-Street Bike Lanes (Class II), Existing
 On-Street Bike Lanes (Class II), Planned
 Signed Bike Routes (Class III), Existing
 Signed Bike Routes (Class III), Planned

 New Traffic Signal

Additionally, green bicycle lanes will be used near all major intersections and driveways.

Figure 6.
Bicycle Circulation Plan

Source: Sonoma County PRMD GIS, USGS NHD;
 Parcels v10; map date: August 2, 2017.
 Revised: February 6, 2019.

Table 4: Bicycle Facility Improvements

Bicycle Lane Safety Markings	
Green Bike Lanes	Use at locations where vehicle and bike traffic interact, such as near intersections and major driveways
Bicycle Lane Buffers	Highway 12: Convert existing 8-foot wide bike lanes to 5-foot wide bike lanes with a 3-foot striped buffer between bicycle and vehicle lanes
Bike Paths and Routes	
New Off-Street Bike Paths	<ul style="list-style-type: none"> ▪ West Thomson Avenue between Happy Lane and Highway 12 ▪ West of Highway 12 between Encinas Lane and Main Street ▪ Verano Avenue between Main Street and Highway 12 ▪ West end of Encinas Lane between Fairview Lane and Encinas Lane ▪ North end of Happy Lane between Orchard Avenue and Happy Lane
New On-Street Bike Routes (“Bike Route” Sign Posted)	<ul style="list-style-type: none"> ▪ Vailetti Drive, between Highway 12 and Lake Street ▪ Lichtenberg Avenue ▪ Boyes Boulevard, between Highway 12 and Greger Street ▪ Melody Lane ▪ Encinas Lane

IV. AUTOMOBILES

The recent improvements to Highway 12 and existing development patterns limit the opportunities to significantly improve automobile circulation in the Springs. However, reducing the number of driveways and consolidating access to parcels would improve vehicle travel through the Springs and increase pedestrian safety. Driveways should be consolidated or removed as new development and redevelopment occurs. Parcels should also be accessed through side streets or alleys, whenever feasible. Strategies to improve vehicle travel and parking in the Springs are summarized in Table 5.

Parking. The Specific Plan provides several strategies to increase parking, resulting in approximately 207 to 322 new parking spaces. A large parking lot is planned to be located in the vicinity of the Springs Plaza and, depending on the specific location, would accommodate 100 to 215 spaces. A second surface lot would be created in the northern area to accommodate approximately 35 spaces. Additional local street parking would be added on Lichtenberg, Hawthorne, and West Thomson Streets to provide approximately 43 new parking spaces.

New on-street parking is also planned for two segments of Highway 12, creating approximately 9 new spaces between Waterman and Central Avenues, and approximately 20 new spaces between Calle del Monte and W. Thomson Avenue. These spaces would be created in areas where the center turn lane on the highway plays a less critical role and can be converted to a painted median, shifting some of the roadway width to create room for parking on one side of the street. Left-turns to and from private driveways along Highway 12 would be prohibited on both of these segments; though full vehicle access would still be available from parallel streets or alleys. Left-turn lanes at public street intersections would be maintained, as would the on-street bicycle lanes. The locations of new on-street parking facilities are shown on Figure 7.

New on-street parking would also be created along local streets as parcels are redeveloped. These additional spaces are not included in the 207 to 322 new spaces described above.

3-10	Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20
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Table 5: Automobile Circulation and Parking Improvements

Highway 12	
Driveway consolidation	<ul style="list-style-type: none"> Consolidate driveways New development is required to provide rear access and eliminate driveways on the highway whenever possible
Left turns at Intersections	Left turns permitted at all public street intersections except Arroyo Road, to allow for a new crosswalk with median refuge at that location
Left Turns at Driveways	Use painted median to prohibit left turns to and from private driveway in areas with on-street parking
New traffic signals	<ul style="list-style-type: none"> Install a traffic signal at Highway 12/Calle del Monte Install a traffic signal at Highway 12/Donald Street
Signal timing	Coordinate timing of traffic signals along Highway 12 within the Plan area
Restriping	Restripe as shown in Figure 8
Side Streets	
Typical cross-section	<p>See Figure 10 for Lichtenberg and Hawthorne Avenues</p> <p>See Figure 11 for West Thomson Avenue</p> <p>See Figure 12 for other local streets</p>
New traffic controls	Install all-way stop controls or mini-roundabout at Donald Street/Robinson Road
On-Street Parking	
Side Streets	<p>Add parking on the following side streets:</p> <ul style="list-style-type: none"> Lichtenberg (6 spaces) Hawthorne (12 spaces) West Thomson (25 spaces)
Highway 12	<p>Add spaces to one side of Hwy 12 in the following areas:</p> <ul style="list-style-type: none"> Waterman to Central (approx 9 spaces) Calle del Monte to West Thomson (approx 20 spaces)
Surface Lot(s)	<ul style="list-style-type: none"> Surface lot in vicinity of Boyes Blvd (100 to 215 spaces) Surface lot in vicinity of Flowery School (35 spaces) Add surface lot in central area (100 to 215 spaces)

V. TRANSIT

The Springs Specific Plan includes policies designed to improve transit ridership. The Specific Plan calls for Sonoma County Transit to increase the frequency of service between the Springs and the City of Sonoma (Route 32). The Plan also recommends that this local route be rebranded as a shuttle service with a distinct look to the buses and signs to increase public awareness of the route.

Bus shelters with benches, route information, bike racks, and lighting would be added along Highway 12. Pedestrian access to the transit system would be enhanced by sidewalk improvements and crosswalk amenities, such as bulb-outs and warning lights near bus stops.

Table 6: Transit Improvements

Transit Facilities	
Improvements	Add shelters, benches, route information, bike racks, and lighting
Pedestrian connections	Prioritize pedestrian crossing amenities near transit stops
Routes	
Increased frequency	Work with Sonoma County Transit to increase the frequency of service between the Springs and the City of Sonoma (Route 32) and the regional transit routes operating between Santa Rosa and Sonoma (Routes 30 and 34).
Marketing and Awareness	Work with Sonoma County Transit to continue the public awareness campaign for the bus route that travels between the Springs and the City of Sonoma (Route 32) to encourage ridership.

3-12	Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20
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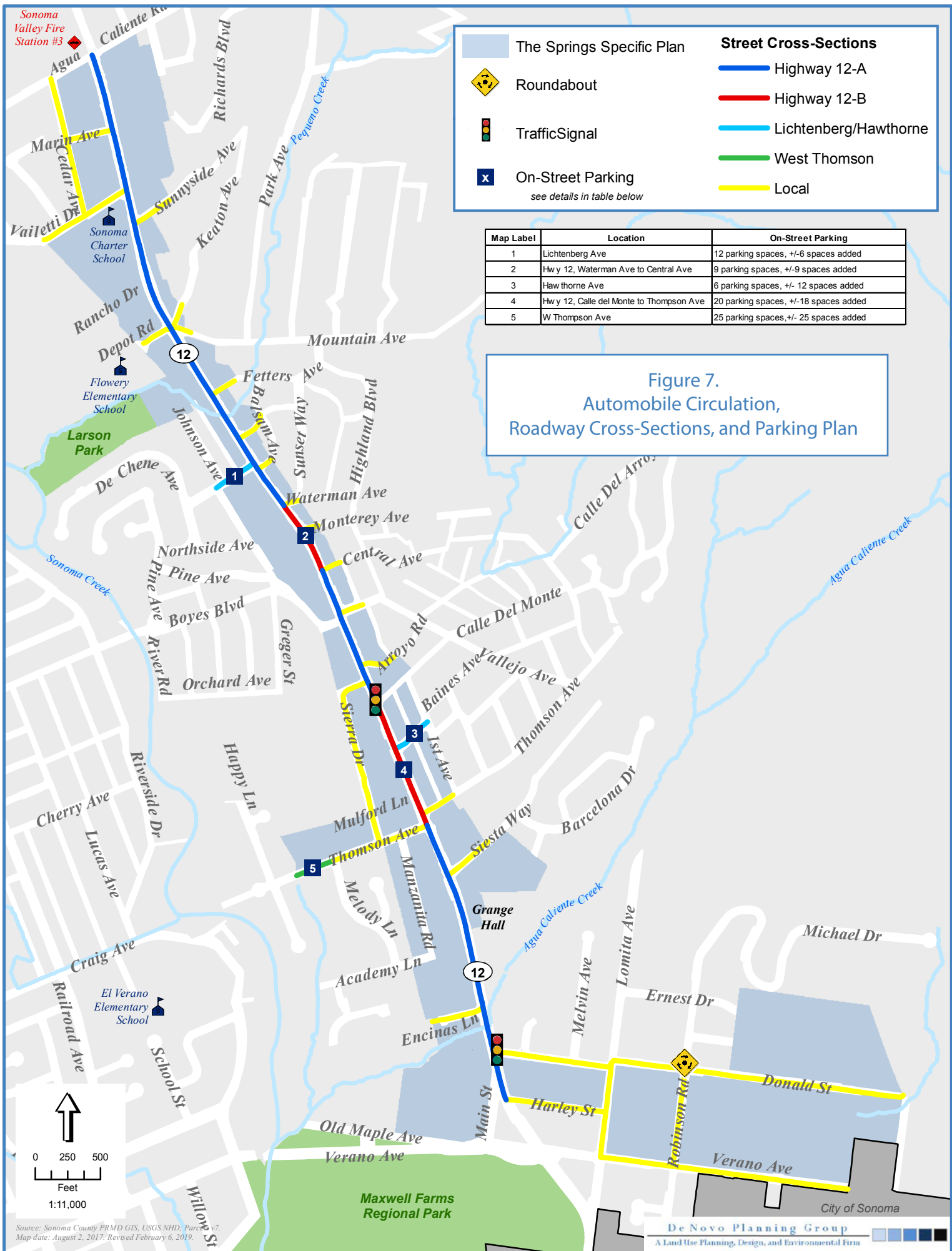
VI. ROAD STANDARDS

Highway 12 is the primary roadway in the Springs Specific Plan area. The Specific Plan includes modifications to the cross-section of Highway 12 to increase bicycle safety and improve the pedestrian experience (see Figures 8 and 9). The Plan also includes modifications to Lichtenberg, Hawthorne and West Thomson Avenues to increase the supply of on-street parking (see Figures 11 and 12). Finally, the Plan provides a cross section for the remaining local side streets. See Figure 7 for cross section locations.

Highway 12

Two distinct cross sections have been developed for the Highway 12 corridor. Cross Section A is similar to the existing Highway 12 configuration and is used in segments where a center turn lane is needed. Cross Section B is used in locations where on-street parking on one side of the street is desired and left-turns to and from private driveways can be eliminated due to the presence of parallel streets and alleys that provide an alternate means of access. See Figure 7 for Highway 12 cross section locations.

Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20	3-13
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The Springs Specific Plan

Street Cross-Sections

- Roundabout
- Traffic Signal
- On-Street Parking
see details in table below
- Highway 12-A
- Highway 12-B
- Lichtenberg/Hawthorne
- West Thomson
- Local

Map Label	Location	On-Street Parking
1	Lichtenberg Ave	12 parking spaces, +/- 6 spaces added
2	Hwy 12, Waterman Ave to Central Ave	9 parking spaces, +/- 9 spaces added
3	Hawthorne Ave	6 parking spaces, +/- 12 spaces added
4	Hwy 12, Calle del Monte to Thompson Ave	20 parking spaces, +/- 18 spaces added
5	W Thompson Ave	25 parking spaces, +/- 25 spaces added

Figure 7.
Automobile Circulation,
Roadway Cross-Sections, and Parking Plan

Source: Sonoma County PRMD GIS, USGS NHD, Parcels v7.
Map date: August 2, 2017. Revised February 6, 2019.

Highway 12 Cross Section A

Wide Sidewalks. Cross-section A features a 9.5-foot wide sidewalk on each side of the street, which is 4.5 feet wider than the existing sidewalk. The additional 4.5 feet of sidewalk width will be provided incrementally as new development takes place.

Bicycle Lanes. The 5-foot wide bike lane is separated from driving lanes by a 2.8-foot painted buffer strip. Green bike lanes would be used near intersections and other locations where bike and auto traffic interact.

Center Turn and Driving Lanes. No changes are proposed to the width of either the existing driving lanes or the center turn lane.

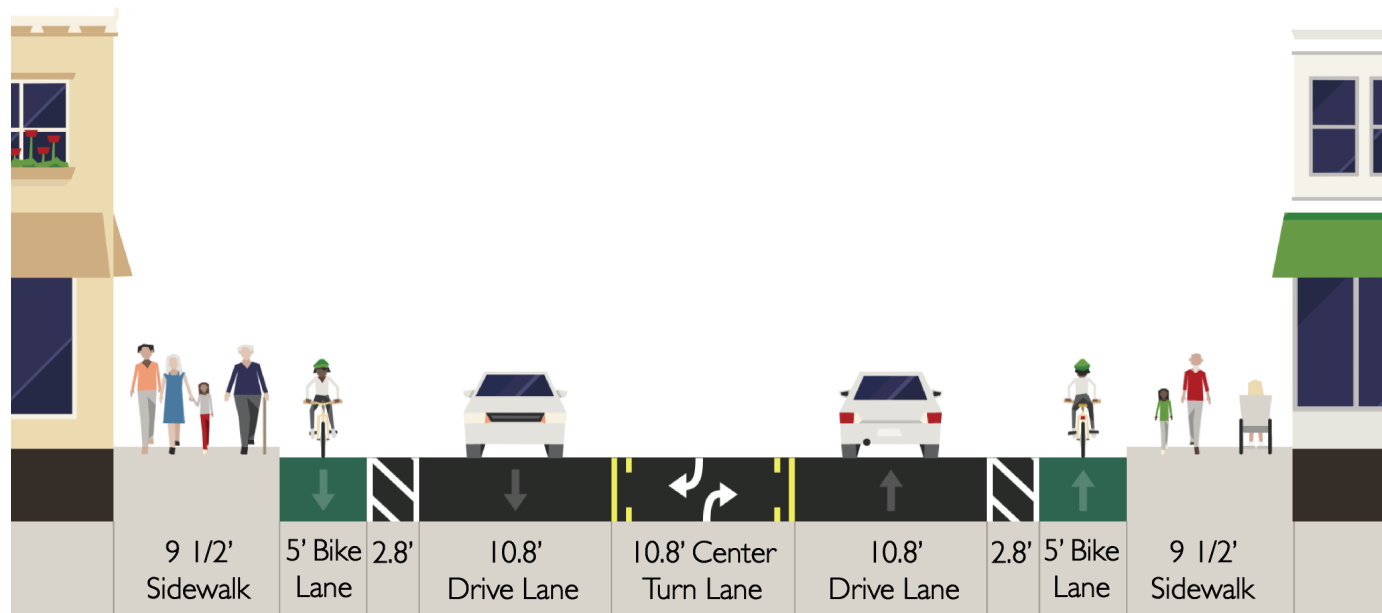


Figure 8: Highway 12 Cross Section A (Center Lane, No Parking)

Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20	3-15
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Highway 12 Cross Section B

Wide Sidewalks. Cross-section B is similar to Cross-section A in that it features 9.5-foot wide sidewalks, which are 4.5 feet wider than the existing sidewalk. The additional 4.5 feet of sidewalk width will be provided incrementally as new development takes place.

Street Parking. Cross-section B also features an 8-foot wide parking lane on one side of the street.

Bicycle Lane and Buffer. A 2-foot wide striped buffer would be provided between the bicycle lane and the driving lane. Green colored bike lanes would be located in areas where bicycle and auto traffic interact, such as at intersections.

Median. A 4-foot raised median, would be provided between opposing traffic lanes to prevent left turns. Cross-section B would be modified at public intersections as needed to provide left-turn pockets on Highway 12. On-street parking would be discontinued in these areas to provide space for a left-turn lane.

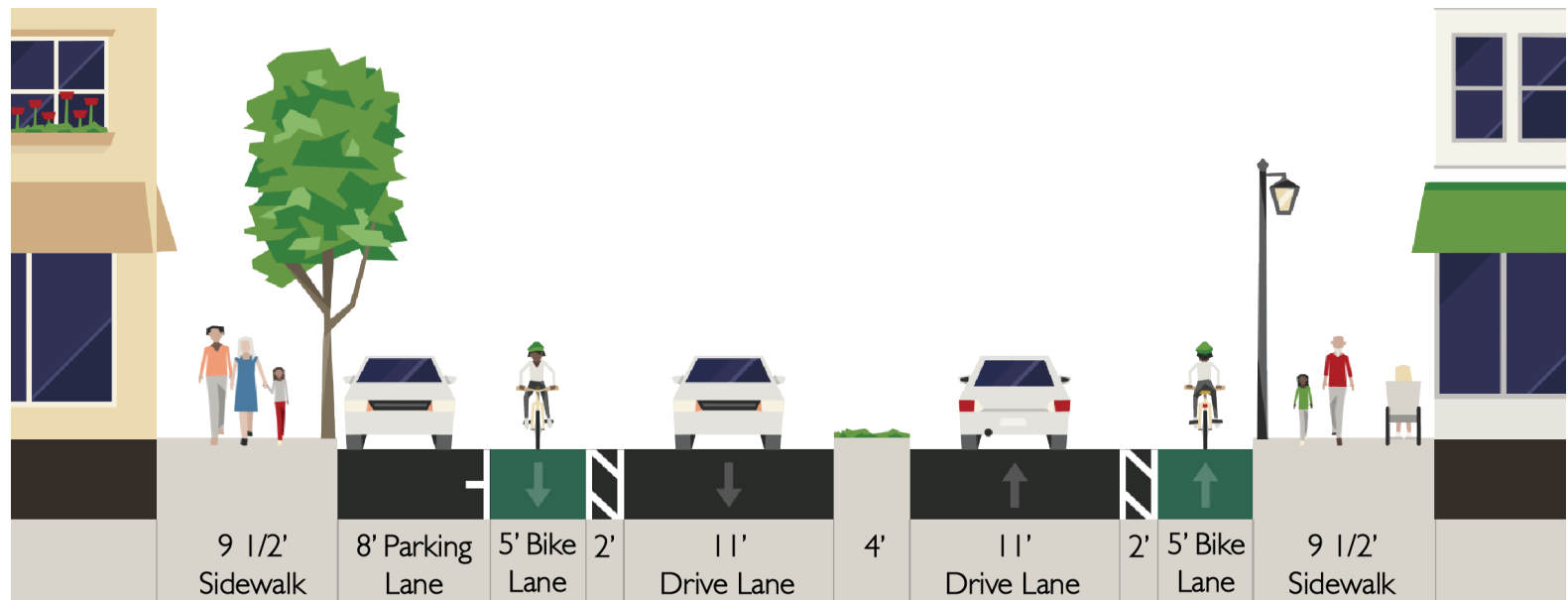


Figure 9: Highway 12 Cross Section B (No Center Lane, Parking on One Side)

Note: Implemented when driveway access is eliminated in the locations for this cross-section (Figure 7).

3-16	Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20
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Lichtenberg Avenue/ Hawthorne Avenue

Lichtenberg and Hawthorne Avenues have 60-foot right-of-ways. These roadways should be reconfigured to include 5-foot wide sidewalks with 5-foot landscape areas to separate pedestrians from vehicles. The 40-foot wide paved street accommodates bidirectional vehicle travel lanes and on-street parking on both sides of the street.

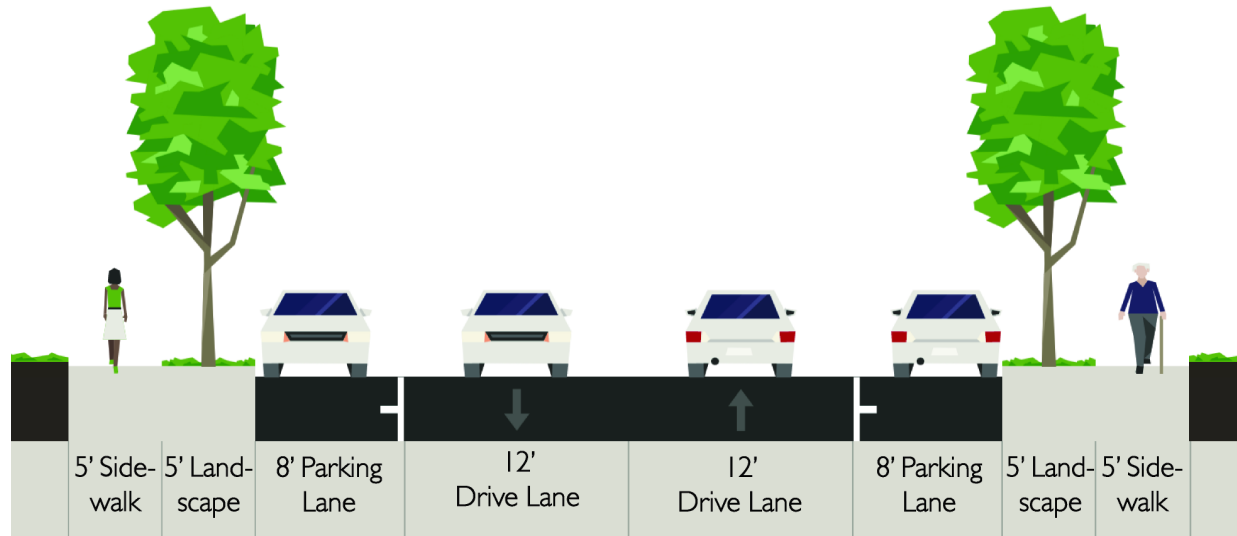


Figure 10: Cross Sections for Lichtenberg and Hawthorne Avenues (60-foot Right-of-Way)

Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20	3-17
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West Thomson Avenue

The segment of the West Thomson Avenue between Melody Lane and Happy Lane is part of the planned Central Sonoma Valley Bikeway. The north side of the street, from Highway 12 to Happy Lane, will be improved with an 8- to 11-foot wide multi-use path and a 5-foot wide planting strip.

This configuration would result in approximately 25 new spaces on West Thomson Avenue.

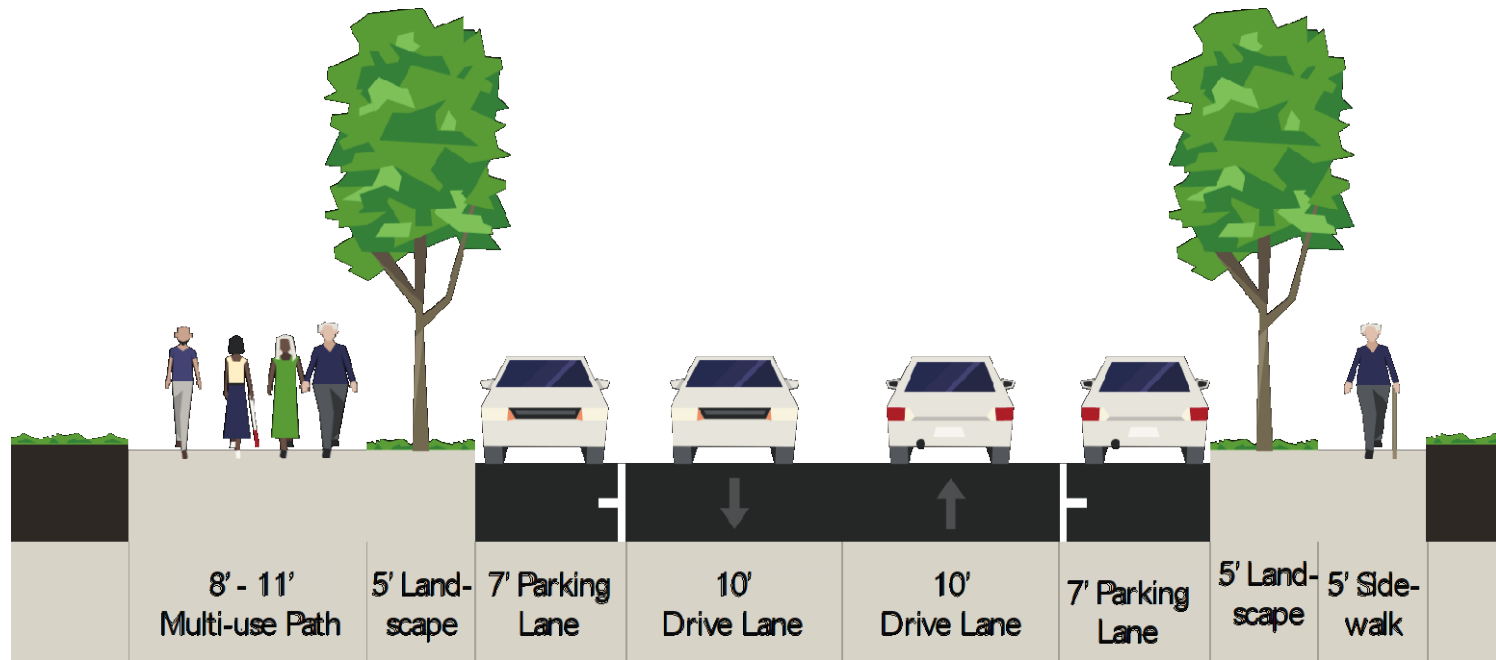


Figure 11: Thomson Avenue Cross Section (60-foot Right-of-Way)

Note: The 8- to 11-foot sidewalk is part of the Central Sonoma Valley Bikeway.

3-18	Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20
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Local Streets

Local streets in the Springs area currently have many different widths, configurations, and building setbacks. New development and redevelopment projects should be designed to achieve the minimum roadway standards shown below.

The minimum standards include a 5-foot wide sidewalk with a 4-foot planting strip to separate pedestrians from vehicles. The 26-foot wide paved street standard accommodates bidirectional vehicle travel and on-street parking on one side of the street. On-street parking or the landscaping zone may be reduced or eliminated in areas with physical or environmental constraints, such as existing buildings, creeks, and significant trees.

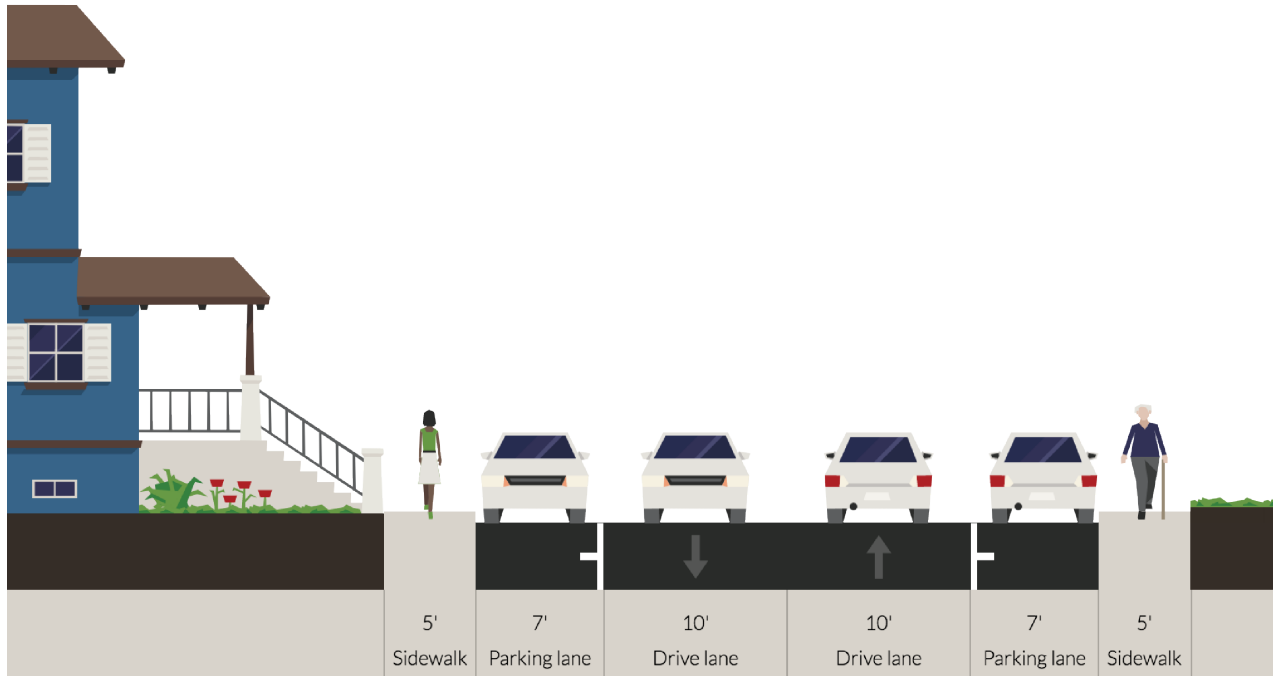


Figure 12: Local Streets, Typical Cross Section for 44-foot Right-of-Way

Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20	3-19
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VII. CIRCULATION GOALS & POLICIES

Goal SC-1: Ensure that the Street Network is Designed to Provide Equally for the Needs of All Users, including Pedestrians, Bicyclists, Motorists, and Transit Riders.

Policy SC-1a: Make it easier and safer to get around the Springs by foot, bicycle, transit, and automobile.

Policy SC-1b: Ensure that circulation improvements result in attractive, functional roadways, bicycle lanes, sidewalks, pathways, transit stops, and parking areas that enhance access and safety for all users.

Policy SC-1c: Continue to improve and enhance Highway 12 to create a vibrant, multi-modal corridor by requiring wider sidewalks, buffered bike lanes, shade trees, street furniture, and other amenities.

Policy SC-1d: Improve traffic flow by decreasing the number of driveways along Highway 12. Consolidate driveways whenever possible and provide access to parcels via side or rear streets or alleys.

Policy SC-1e: Implement the roadway cross-sections included in this Specific Plan which are designed to accommodate all modes of transportation including walking, bicycling, transit, and driving.



Water filling stations encourage healthy hydration and reduce the use of plastic water bottles.

3-20	Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20
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Policy SC-1f: Coordinate with Caltrans and the City of Sonoma to consider the potential redesignation of Highway 12 to parallel routes that are better-suited to accommodate regional traffic.

Policy SC-1g: Monitor traffic patterns on Highway 12 and collaborate with Caltrans periodically to adjust traffic signal timing to improve the flow of traffic.

Policy SC-1h: Development projects that exceed ten (10) residential units or 5,000 square feet of non-residential development shall reduce VMT through implementation of a Transportation Demand Management (TDM) plan. Development projects shall be subject to the TDM conditions below, which require applicable projects to provide a foundational set of strategies plus one additional measure. A project may propose construction or funding of offsite pedestrian, bicycle, and transit infrastructure and/or participation in future regional or countywide VMT reduction programs, in lieu of a TDM plan if demonstrated to the satisfaction of the PRMD Director that the associated reduction in vehicle travel would be comparable to the TDM requirements.

- A. Foundational Measures: Development projects must implement all of the following TDM measures at a minimum:
 - On-site or contracted TDM coordinator
 - TDM marketing
 - Rideshare matching
 - Onsite bicycle amenities
 - Emergency Ride Home Program (applies to nonresidential uses)
- B. Additional Measures: Development projects must implement at least one additional TDM measure. The measure must be approved by the County and can be chosen from the strategies below. The enumerated list does not preclude a project from implementing other TDM measures if desired or required by County Code.

Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20	3-21
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Nonresidential development

- Transit/vanpool subsidies
- Parking cash-out
- VMT Mitigation Bank (if available)
- Off-Site Physical Non-Auto Mode Improvement(s)

Residential development

- Transit subsidies
- School-pool matching
- Unbundled parking
- VMT Mitigation Bank (if available)
- Off-Site Physical Non-Auto Mode Improvement(s)

3-22	Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20
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Goal SC-2: Create a Safe, Convenient, and Well-connected Pedestrian and Bicycle Circulation System with Generous Amenities that Encourage Walking and Cycling.

Policy SC-2a: Ensure that circulation improvements create a walkable and bikeable community with convenient access to schools, parks, shops, services, restaurants, and other local destinations.

Policy SC-2b: Improve pedestrian and bicycle linkages and facilities throughout the Springs to improve mobility; provide safe routes to schools and transit stops; make the area more inviting to pedestrians and cyclists; and improve connectivity to nearby communities and regional destinations. See Figures 5 and 6 and Tables 3 and 4.



Intersection with pedestrian refuge, enhanced crosswalk markings and texture, and green bike lanes.

The ultimate configuration of any new pedestrian crossings shall be evaluated and determined by the Sonoma County Department of Transportation and Public Works, in collaboration with Caltrans on crossings along Highway 12, and in consideration of the physical characteristics and best design practices that exist at the time the design is initiated.

Policy SC-2c: Create a pedestrian- and bicyclist-friendly environment by ensuring that new development is human-scale and areas are provided for public seating. Other amenities that should be provided include street furniture, landscaping, shade, bicycle racks, trash receptacles, and pedestrian oriented lighting and signage. Amenities should be placed in locations that do not decrease the walkability of the sidewalk.

Policy SC-2d: Require that adjacent developments be connected by safe, direct walkways. Ensure that projects are designed to anticipate and accommodate future street and sidewalk connections to new development on adjacent lands.

Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20	3-23
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Policy SC-2e: Prohibit cul-de-sacs and dead end streets, except where existing conditions require them. If cul-de-sacs are necessary, require walkways connecting to adjacent streets and future development.

Policy SC-2f: Require direct pedestrian access between housing and any adjacent transit facility.

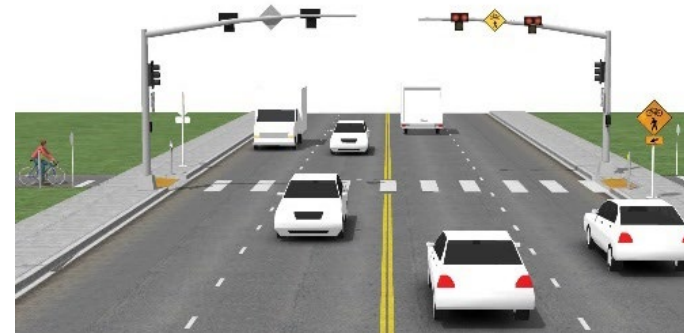
Policy SC-2g: Provide new and improved crosswalks as shown in Figure 5. Prioritize safety features, such as pedestrian warning lights and bulb-outs, that improve visibility and create a more comfortable pedestrian environment, particularly in the vicinity of schools and parks.

Policy SC-2h: Provide new and improved bicycle lanes and enhance bicycle safety through the use of signs, bicycle lane buffers, and green colored pavement, as shown in Figure 6. Priority should be given to intersections when making safety improvements.

Policy SC-2i: Prioritize crosswalk, sidewalk, and bicycle lane improvements near schools, parks, transit stops, and the Springs plaza.

Policy SC-2j: When planning new crosswalks, locate crosswalks on the far side of the bus stop so that the bus passes through the crosswalk before stopping for riders.

Policy SC-2k: Require development projects along Highway 12 to provide increased sidewalk widths, consistent with the cross-sections identified in this chapter and the setback requirements set forth in the Design Guidelines chapter.



Pedestrian Hybrid Beacon at Midblock Crossing (Source: NACTO)

3-24	Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20
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Benefits of Street Trees

- *Provide shade and reduce the urban heat island effect*
- *Encourage physical activity*
- *Clean the air*
- *Reduce greenhouse gases*
- *Improve water quality*
- *Increase visual interest*
- *Buffer pedestrians from adjacent vehicles*

Policy SC-2l: Establish an improvement district or comparable mechanism to fund installation and maintenance of water stations, benches, street trees, landscaping, trash cans, and other community amenities along the Highway 12 corridor.

Policy SC-2m: Require development projects to establish a mechanism to fund landscaping and maintenance of the required landscaping section along Lichtenberg Avenue, Hawthorne Avenue, and W. Thomson Street.

Policy SC-2n: Require new development and redevelopment projects to include street trees that will provide a shaded canopy whenever possible.

Where street canopy trees are not feasible due to underground infrastructure or other issues, non-canopy trees or other street landscaping, such as planters, may be used, or the street trees may be set back from the sidewalk on private property.

Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20	3-25
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Policy SC-2o: Encourage the development of public spaces, such as outdoor seating areas, that are easily accessible from the public sidewalk or pathway. Ensure that public spaces are designed for pedestrian comfort and provide visual interest.

Policy SC-2p: Provide water filling stations at key locations along the Highway 12 corridor. Recommended locations are shown on Figure 6, Bicycle Circulation Map.



Street trees provide pedestrians comfort, offering shade from the sun and protection from the wind. Street trees, landscaping, and human-scale amenities create a bicycle and pedestrian friendly environment. (Source: Caltrans Main Street Guide)

3-26	Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20
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Goal SC-3: Increase Transit Ridership in the Springs Area



Sonoma County Transit bus (Source: Sonoma County Transit)

Policy SC-3a: Coordinate with Sonoma County Transit to improve local bus service by increasing the frequency of bus service in the Springs and decreasing travel times.

Policy SC-3b: Support the creation of a public awareness campaign to promote transit use. Provide easy to understand schedule and bus pass information in English and Spanish.

Policy SC-3c: Coordinate with Sonoma County Transit to promote the local shuttle service (route 32) which runs between the Springs and the City of Sonoma, including continuing the branding of route 32 as a shuttle, creating a distinct look for shuttle vehicles, and updating transit signage for route 32. Sonoma County Transit is also encouraged to allocate marketing resources to publicize the shuttle route to residents, employees, and visitors.

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- Policy SC-3d* Work with Sonoma Transit to improve bus stops by providing well-lit shelters, benches, bicycle racks, and trash cans. Provide schedule information at each bus shelter location.

- Policy SC-3e:* Utilize public art at bus stops and unique designs for street furniture, recognizing that all bus shelter structures will be designed according to Sonoma County Transit’s standards.

- Policy SC-3f:* In conjunction with road or development projects, review whether a bus turnout is appropriate in locations where transit shelters exist or are planned.

- Policy SC-3g:* Maintain fare-free service on the Sonoma County Transit local route serving the Springs area (currently route 32 Sonoma Shuttle).

- Policy SC-3h:* Explore use of micro-transit and on-demand transit.

- Policy SC-3i:* Encourage private shuttles to serve the community.

- Policy SC-3j:* Work with local employers and retailers to identify opportunities for private shuttles to serve employment sites and other destinations that are not currently served by transit.



Springs area bus stop with shelter and schedule information.

Goal SC-4: Ensure Adequate Public and Private Parking to Accommodate Residents, Businesses, and Visitors to the Springs

Policy SC-4a: Facilitate the development of public parking lots in proximity to the future community plaza (Highway 12/Boyes Avenue) and in the northern portion of the mixed use corridor, as described in Table 5. Integrate retail into the street-level frontage of any parking garages constructed in a commercial district.

Policy SC-4b: Minimize the negative impacts of parking on the overall site design of individual projects by locating parking to the rear of the site, either behind or below buildings, unless parking is provided in a multi-level structure or a shared parking facility. Parking for parcels located along the Highway must be accessed from either side or rear streets or alleys whenever possible. If the site does not have a rear or side street access, shared driveways should be used to minimize sidewalk disruption.



Above-ground parking structure with façade designed to complement nearby buildings.

Policy SC-4c: Encourage parking to be located within or beneath buildings.

Policy SC-4d: Support car-sharing by encouraging larger development projects to reserve parking spaces for car-share vehicles. Reserve strategic on-street spaces for car-share vehicles as demand for such services increases.

Policy SC-4e: Encourage the development of shared parking facilities among uses that experience different peak use periods.

Policy SC-4f: Support and recognize shared parking arrangements created by local businesses that provide jointly owned or leased parking lot space to meet the parking requirements of two or more businesses.

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Policy SC-4g: Allow the on-street parking spaces located immediately adjacent to a business to be counted towards that business’ parking requirement. Parking along the street frontage of a multi-tenant building may be credited to each individual business based on the percentage of the building occupied by each use.

Policy SC-4h: Reduce parking requirements where there are established shared parking areas that can accommodate peak parking demand. A parking analysis may be required.

Policy SC-4i: Consider the establishment of a parking district or in-lieu parking fees to fund the construction of new public parking and programs that reduce parking demand, such as bicycle path development and transit improvements.

Policy SC-4j: Encourage the installation of electric charging stations on both public property and in private development.

Policy SC-4k: Allow residential development to use tandem parking spaces (where one car parks behind another) to meet parking requirements.

Policy SC-4l: Require bicycle parking near the front entrance of commercial buildings.

Policy SC-4m: Include bicycle parking in all parking lots and structures.



Bicycle parking example.

3-30	Overview 3-1	Pedestrians 3-2	Bicyclists 3-7	Automobiles 3-10	Transit 3-12	Road Standards 3-13	Goals & Policies 3-20
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I. INTRODUCTION

A. Purpose

The purpose of the Springs Design Guidelines is to promote an attractive, vibrant, and well-connected community that entices people to walk, bike, and enjoy the outdoors. The Design Guidelines are intended to facilitate well-designed projects tailored to reflect the community’s rich history and harmonize with its notable architectural styles.

B. Applicability

These guidelines apply to all non-residential, mixed-use, and residential development projects that involve:

1. New construction
2. Additions
3. Exterior modifications
4. New signs

C. Implementation

The design guidelines provide a fundamental framework for superior project design and are used by the Design Review Committee and staff as a tool to evaluate projects.

The decision-making body, such as the Planning Commission, Design Review Committee, or, for ministerial and minor entitlements, County staff, may approve departures from the design guidelines when the applicant has demonstrated that an alternative approach would accomplish the following:

Important Terms

“Shall” and “Must”
Guidelines that use the words “Shall” or “Must” are mandatory.

“Should”
Guidelines that use the word “Should” are intended to be applied as stated. However, an alternative measure may be considered if would result in a superior project and meets or exceeds Design Guideline objectives.

“Encouraged” and “May”
Guidelines that use the word “Encouraged” or “May” are recommended but not mandatory.

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1. Result in a substantially superior project;
2. Demonstrate creative and innovative architecture and design;
3. Harmonize with the natural setting and built environment of the Springs;
4. Enhance the visual quality of the Springs community,
5. Contribute to an inviting, pedestrian and bicycle-oriented atmosphere; and
6. Accomplish the objectives included at the beginning of each section of these Design Guidelines.

A request for departure from the guidelines must accompany the project application and specifically describe how the project accomplishes the criteria set forth above.

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II. BUILDING CHARACTER

A. Design

***Objective 1:** Ensure that new and renovated buildings are designed to enhance the built environment, complement the surrounding uses, and harmonize well with the few iconic buildings that remain in the Springs.*

***Objective 2:** Create an attractive and inviting pedestrian-oriented environment featuring well-designed buildings, active storefronts, and a pedestrian scale.*

1. **Harmonize with Iconic Architecture.** The architectural style of new and renovated buildings must harmonize well with the iconic architecture found in The Springs. Iconic architectural styles of The Springs include Mission Revival, Mid-Century Modern, and Vernacular Commercial.
 - a. Contemporary adaptations of the Iconic and vernacular commercial architecture of the Springs are encouraged.
2. **Complement Surrounding Uses.** New and renovated buildings must be designed to complement the surrounding environment and fit well with adjoining development.
3. **Four-sided Architecture.** Buildings must be designed to be aesthetically pleasing from all angles. All sides of new and renovated buildings shall exhibit high quality design, variations in massing and wall planes, and architectural features and detailing. Blank, featureless walls are not permitted.
4. **Pedestrian Scale Design.** All new development must be designed to achieve a pedestrian scale.

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Mission Revival

The Mission Revival style was one of the most popular turn-of-the-century architectural movements in California. In recent decades, the style has experienced resurgences in popularity. The Mission Revival style is influenced by California's early architectural tradition, particularly Spanish missions, and romanticized picturesque aesthetics. Mission Revival was seen as fashionable and eclectic, often reflecting a connection between the outdoors and the building's interior.



The Sonoma Mission Inn exhibits classic Mission Revival style, with its bell tower, inset windows and doors, arched entries, and clay roof tiles.

The style's defining architectural features include:

- a. Mission-shaped parapet
- b. Wide, overhanging eaves with decorative brackets
- c. Mansard (sloped) roof or shallow pitched roof with gabled or flat ends and red clay tile
- d. Trimless windows with expressed sills
- e. Arched and/or inset windows and doors
- f. Hand-formed stucco walls
- g. Quatrefoil windows
- h. Minimal surface decoration
- i. Decorative glazed tile wainscoting
- j. A bell tower



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Vernacular Commercial

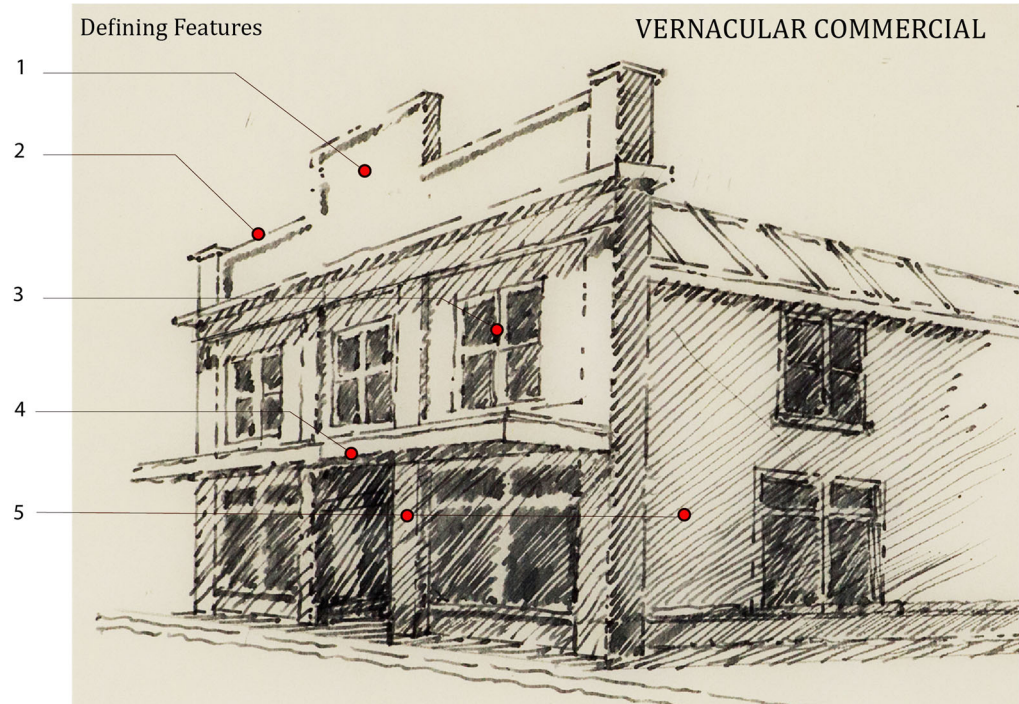
The Vernacular Commercial (also referred to as Western False Front) style reflects the limitations posed by frontier communities' early settlement conditions, such as lack of access to technologies necessary to create refined building materials, such as bricks. This style is defined by a tall, flat front façade that conceals the simple gable end of a narrow building, providing a more significant presence facing the street and maximizing the area for signage. The style included the use of naturally occurring materials that required minimal processing. While the style is considered informal and allows for modification, non-historical features that should be avoided include: decorative trim, cornice brackets, and brick, tile, or stone wainscoting.



The Barking Dog exhibits elements of the Vernacular Commercial style, with a flat front, decorative façade. The stone tiles are a modern addition inconsistent with the style.

The style's defining architectural features include:

- a. A flat front façade that rises to a parapet
- b. Stepped cornice line
- c. Wooden sash windows
- d. A front canopy, typically supported by wood columns
- e. High quality materials and increased ornamentation on the front of the building
- f. Wooden doors that may be glazed
- g. Wooden siding with vertical battens (most historically accurate)



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Mid-Century Modern

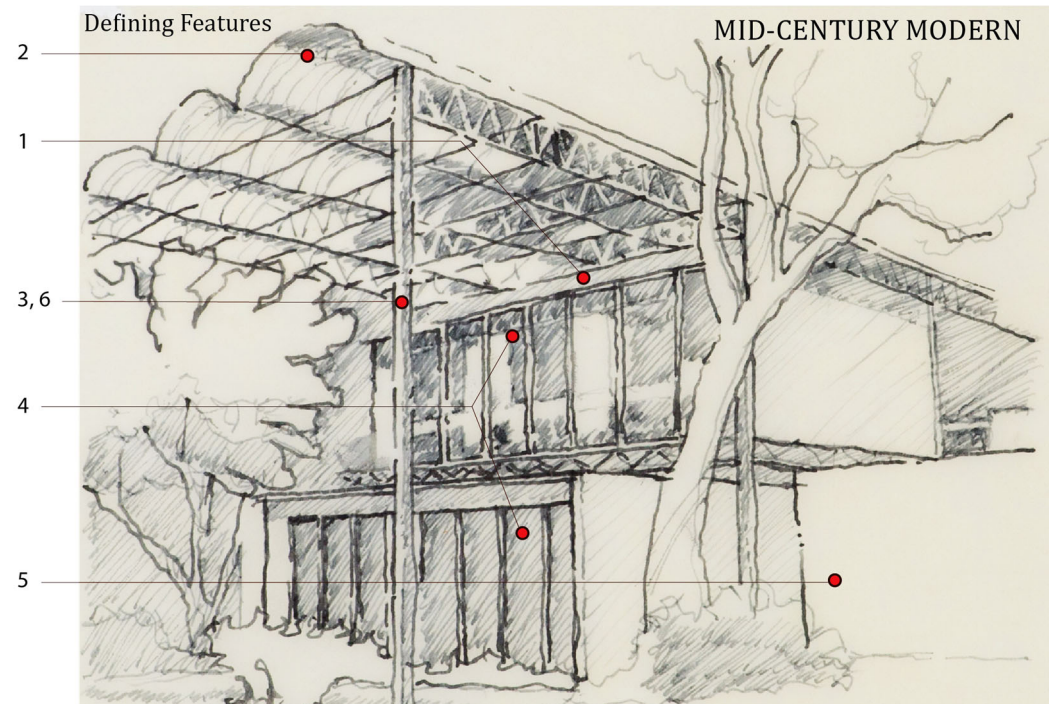
The Mid-Century Modern is characterized by clean, simple design and integration with nature. This style emphasizes structures with ample windows, strong vertical and horizontal planes, and open floor plans. The post and beam system is often used to reduce bulky supporting walls and to accommodate extensive expanses of glass.



The Ross Drulis Cusenberry building expresses vertical and horizontal elements, clean lines, and floor to ceiling windows typical of Mid-Century Modern.

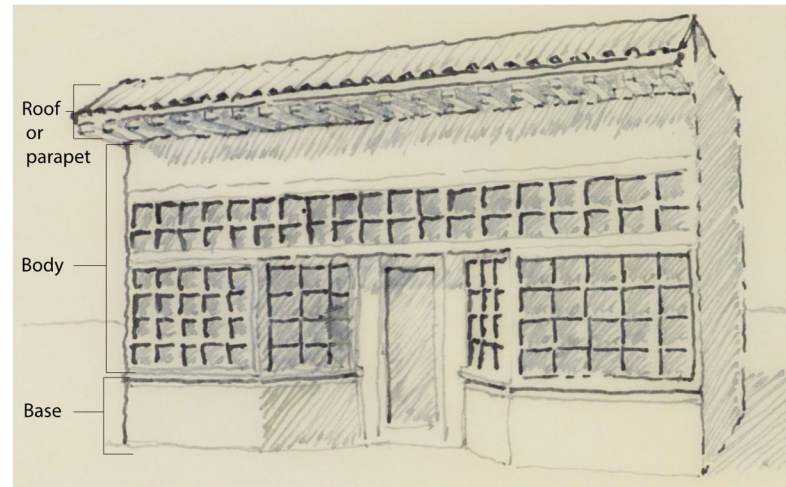
The style's defining architectural features include:

- a. Thin plane appearance of flat, low slope or folded plate roofs
- b. Metal screens/brise soleil
- c. Strong expression of vertical and horizontal elements, clean lines
- d. Floor to ceiling windows
- e. Atria or courtyards, Integrating with nature
- f. Minimal ornamentation, with the exception of decorative screens or repeated simple elements



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- 5. **Building Base, Body, Roof.** The design of new and renovated commercial structures should include a well differentiated base, body, and roof.
- 6. **Variations in wall plane (modulation).** The design shall create variations in wall surfaces to create varied massing, a sense of depth, and a pedestrian scale. This can typically be addressed through the use of recesses, or by setting a portion of the wall back, or by projecting a section of the wall forward a distance of at least one foot.
- 7. **Building Width.** New development must be designed to contribute to a traditional rhythm along the street frontage of 25- to 30-foot-wide buildings. Wider buildings must be architecturally divided into smaller components to give the appearance of a series of smaller buildings. Vertical variations in the wall plane (projections and recesses), along with architectural elements such as pilasters, can be used to create smaller bays.



Traditional commercial storefronts were frequently designed to incorporate an articulated base, body, and roofline. In single-story structures, the base is often a knee-high design element.

This arcade provides protection from the elements, while the recessed area and vertical elements create a pedestrian-friendly scale.



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The second story of this two-story building is set back to reduce the overall mass of the building. Balconies on the second floor overlook the street.

8. **Three-Story Buildings.** The third story of any building that fronts onto a public street must be stepped back at least twelve feet (12') from the lower floor footprint. If there are multiple buildings proposed on a site, three-story buildings should be placed farther from the street than single or two-story buildings to provide a gradient in height from the street to the interior of the project site. The third story façade may include railings to allow for the outdoor use of the recessed area. The use of horizontal detailing (e.g. stringcourse, frieze, etc.) to demarcate floor levels on the exterior of the building is encouraged.
9. **Primary Entrance.** The primary entrance must be prominent, easily identifiable and directly accessible from the public sidewalk. Recessed entries with large display windows are required in commercial areas. Weather protection, such as awnings and arcades, must be provided at building entrances.

4-8	Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-45	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61
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10. **Architectural Features.** Creative, innovative design and architecture is encouraged. Projects must incorporate a range of architectural features and design details, such as those listed below, to create visual interest and pedestrian scale. Buildings with featureless facades are prohibited. Architectural features may include:

- a. Pilasters (shallow column)
- b. Arcade (covered walkway)
- c. Overhangs
- d. Cornices and moldings
- e. Stringcourse (horizontal band projecting from face of building)
- f. Frieze (decorative band)
- g. Lintel (beam above door or window)
- h. Recessed windows or entries
- i. Windows and doors accentuated with trim, sills, mullions, etc.
- j. Large display windows at street level with transom windows above



Example of architectural elements incorporated into building design.

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Example of architectural elements incorporated into building design.

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- 11. **Architectural Consistency (internal).** Architectural elements and details must be consistent with the chosen architectural style and designed to reduce perceived building mass.
- 12. **Blank Wall Treatment.** Blank walls of existing buildings should be enhanced with windows, awnings, canopies, display cases, trellises, vines, murals or architectural details.
- 13. **Street Corner Buildings.**
 - a. New buildings proposed for corner lots shall be oriented towards the street corner.
 - b. A prominent corner entrance must be provided for these buildings.
 - c. The street corner portion of the building must be accentuated with enhanced architectural detailing. Prominent features, such as clock towers and cupolas, are encouraged.
- 14. **Franchise Architecture.** Franchise or trademark architecture is generally intended to be conspicuous and function as advertising. This type of design detracts from the character of the area and shall be avoided. All commercial architecture must be designed to fit the scale and character of the community and be consistent with these guidelines.



This building is oriented toward the street corner with the primary entrance, located on the diagonal, easily visible and accessible. The larger mission façade and mission bell distinguish the entry from other elements of the building.

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A mid-block pedestrian walkway facilitates the use of parking located to the rear of the building.

- 15. **Adaptive reuse.** The adaptive reuse of existing historic and architecturally significant buildings shall be undertaken whenever possible to preserve the historic character of the area.
- 16. **Incorporate Amenities into Design.** Buildings should be designed to include amenities for users and the public, such as protection from the elements and spaces for people to gather and relax.
 - a. Exterior weather protection, such as awnings, overhangs, and arcades, must be provided at building entries. Weather protection should also be provided adjacent to sidewalks and pedestrian areas.
 - b. Courtyards and atriums should be used to bring light and air into interior spaces, where appropriate.
 - c. Amenities must be thoughtfully integrated into building design, rather than added on as an afterthought.
- 17. **Pedestrian Amenities.** The ground floor of buildings shall be designed to encourage pedestrian activity by providing amenities such as seating areas, outdoor dining, entry forecourts, formal landscaping, art displays, pedestrian-level lighting, and other pedestrian-oriented amenities.

4-12	Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-45	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61
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- 18. **Building/Sidewalk Interface.** Restaurants, coffee shops, and other food and beverage businesses should be designed to create a friendly and open interface with the sidewalk.
 - a. Operable windows, doors, and sliding walls should be used to open the building to the sidewalk.
 - b. Sidewalk seating and dining is encouraged where sufficient space exists and shall be set back a minimum of five feet from the face of the curb.
 - c. Seating and dining areas must be located so that pedestrian traffic will not be impeded.

- 19. **Roof.** Buildings with flat or low pitched roofs shall have parapets or architectural elements to break up long horizontal rooflines. Parapets shall be finished with cornices, caps, or similar detail to provide a finished look to the roof plane.



Sidewalk amenities, including landscaping, planters, benches, and human-scale lights, provide a pleasant pedestrian experience.

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B. Windows & Doors

Objective: Ensure that windows and doors are designed to complement the architectural character of a building, enhance the visual interest of the street, and create a sense of comfort and security for pedestrians walking along the sidewalk.

1. **Large Windows.** Large storefront display windows should be incorporated into the design of new and remodeled commercial buildings to provide visual interest and reflect the historic context.
 - a. Windows shall comprise at least 50 percent of the ground-level, street-facing elevation of a building. This requirement shall also apply to any side of the building that faces a public space, such as a plaza, outdoor seating and dining area, and pedestrian walkways.
 - b. Windows shall comprise at least 30 percent of the upper-level, street-facing elevation of a building. Upper stories of buildings should have a window-to-wall area ratio that is smaller than that of ground floor storefronts.



Expansive windows contribute to a visually-interesting storefront and sidewalk. These windows are recessed from the wall plane to increase depth and provide relief along the wall's surface.

2. **Clear Glass.** Windows on the ground level of a commercial building shall be transparent to provide a visual connection between the interior of the building and the sidewalk, creating a safe and inviting atmosphere. Reflective, mirrored or tinted glass shall not be used.

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3. Complementary Design and Placement.

- a. The type of windows and doors used must complement the architectural style of the building.
- b. The arrangement and detailing of windows and doors must be appropriate for the architectural design of the building.

4. **Recessed Windows and Doors.** Recessed windows and doors create shadow lines and depth that add relief to the wall surface.

- a. Windows shall be recessed a minimum of three inches and no more than six inches, to discourage sitting, from the exterior wall plane.
- b. Doors shall be recessed a minimum of three inches.
- b. Commercial storefronts must incorporate recessed entries to accentuate the building entrance, maximize window display areas and provide protection from the weather. The recessed entry shall have a floor treatment that complements the building façade.

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5. Proportion & Scale.

- a. Window openings should generally be designed to have greater height than width. Classic architectural proportions are encouraged (e.g. height-width ratio of 8:5).
- b. Windows and door openings must be in scale with the building elevation on which these features appear.
- c. Large expanses of glass are encouraged to be broken into smaller window panes.

6. **Color.** The use of color is encouraged to accentuate window and door casings and trim.

7. **Detailing.** Windows shall be accentuated with casings, sills, trim, shutters, balconies, trellises, flower boxes, or similar treatments to enhance building facades.

8. **Awnings.** Awnings, canopies and arcades should be used to provide protection from the weather, reduce glare, conserve energy and visually enhance the building.



Ground-level windows facing sidewalk and parking area create an inviting street-front for pedestrians and a welcoming façade and the pedestrian-scale awning enhances the building entrance.

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C. *Color and Materials*

Objective: *Ensure that building colors and materials are aesthetically pleasing, authentic, and compatible with the architectural style of the building and the natural scenic backdrop that surrounds the Springs.*

1. **General Concepts**

- a. Colors and materials should respect the architectural style of the building.
- b. Colors and materials must harmonize well with the styles of the Springs community and the natural scenic backdrop.
- c. Colors and materials should be used in an authentic manner, reinforcing the architectural style and overall development concept.
- d. A well-coordinated palette of colors must be used to tie building elements together.
- e. The color palette must complement the type of exterior materials used.
- f. The materials and colors used for additions and renovations to existing structures should complement the original building architecture and color scheme.
- g. Franchise uses shall use alternative color schemes when determined by the County that their standard color scheme would not be complementary to the Springs community.

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2. Accent Colors

- a. Colors and materials shall be used to accentuate windows, doors, trim and other architectural detailing.
- b. Accent colors should complement the base color.

3. Materials.

- a. Buildings must use high-quality, durable materials that retain their appearance over time and convey a sense of permanence and richness.
- b. Buildings shall incorporate a combination of materials to provide relief and texture, and break up wall surfaces.
- c. Changes in exterior materials shall not occur at exterior corners, but should be wrapped around the corner to give the material depth and appearance of a structural function.
- d. Use of excessively reflective building materials, including mirrored glass, is not permitted.



In this example, the color scheme uses a single base color for building walls and a single accent color for windows and doors.

Use of color is encouraged, particularly when paired with a unified theme. In this example, the colorful fruits and vegetable mural ties into the yellow base color and white and aqua accent colors.



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4. **Murals.** A mural is a large-scale painting or mosaic applied directly to the exterior wall of a building or other outdoor surface. A mural is considered an exterior alteration and is subject to design review.
 - a. Murals should not include any logos or advertising and are encouraged to contain community-oriented messages, rather than commercial messages. Text shall be limited to ten percent of the mural area.
 - b. Murals must be created and installed by a qualified artist.
 - c. The colors used should complement the building.
 - d. Materials should consist of durable, graffiti- and weather-resistant materials.

The Grange Hall mural faces Highway 12, and illustrates the agricultural heritage of the Springs. The remainder of the building is composed of simple colors that do not conflict with the mural.



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III. SITE DESIGN

A. *Building Placement & Orientation*

Objective: Ensure that building placement and site design create a continuous frontage that provides visual interest, encourages pedestrian activity, and enhances street vitality and safety.

1. **Orientation.**

- a. Buildings shall be oriented toward the street.
- b. The primary entrance of the building must face the street and be directly accessible from the public sidewalk.
- c. Development located next to the new public plaza site must be designed and oriented to face the plaza, in addition to the street.

2. **Setback Distance**

- a. **Minimum Setback:** New commercial and mixed-use buildings must be setback at least 10 feet from the face of the curb. This creates an additional five feet of space between the sidewalk and the new building, allowing room for wider sidewalks and amenities such as outdoor dining, benches, and planters.
- b. **Maximum Setback:** New buildings may be setback up to 20 feet from the face of the curb to accommodate outdoor dining, seating areas, courtyards, and other public spaces and pedestrian oriented amenities.

3. **Continuous Frontage.** The front elevation of commercial and mixed-use buildings should extend across the entire front of the parcel to achieve a continuous array of storefronts along the Highway 12 corridor, whenever possible. However, a mid-block pedestrian walkway, used to provide access from the sidewalk to a rear parking lot, is encouraged.

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B. Pedestrian Circulation

***Objective:** Create a safe, efficient and attractive network of sidewalks and walkways to encourage greater pedestrian activity.*

1. **On-site Connections.** All new development shall include well-defined pedestrian walkways that create a clear path of travel between parking, buildings, and sidewalks and pedestrian amenities such as outdoor seating and dining areas.
2. **Connect to Adjacent Uses.** All new projects shall be designed to incorporate safe, convenient walkways which connect to adjacent uses and public areas.
3. **Connect to Future Uses.** New development shall be designed to facilitate future pedestrian connections with anticipated development in the surrounding area.
4. **Connect to Rear Parking Area.** Mid-block pedestrian walkways that provide access from the sidewalk to a rear parking lot may be required.
5. **Connect to Creeks.** Where new non-residential development occurs adjacent to creeks, a pedestrian access should be provided to allow pedestrian views of the creek and should include a shaded seating area for public viewing and enjoyment.
6. **Vehicle Separation.** Planters should be provided to separate pedestrian walkways from parking, driveways or roads that abut the walkway.



This pedestrian arcade provides connectivity between the main street and the parking area located at the rear of the commercial buildings.

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- 7. **Amenities.** Amenities, such as special paving, shade, water stations, pedestrian scaled lighting, and benches must be provided along walkways to create a pleasant and inviting pedestrian environment.
- 8. **Tree Grates.** Tree guards, and either grates or precast pavers, must be provided to protect trees from damage and extend the walking surface for pedestrians.

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C. Parking

Objective 1: Minimize the visual impact of surface parking.

Objective 2: Minimize the number of driveways along the Highway 12 corridor to improve both pedestrian and vehicle circulation.

1. **Location.** Onsite parking shall be located to the rear of buildings, unless the parking is podium-style or located underground. Locating parking alongside of a building will only be considered when rear parking is not physically possible. Onsite parking shall not be located in front of the building.
2. **Connect & Share.** Rear parking areas should be interconnected and shared by multiple businesses whenever possible. Cross-parking easements are encouraged for this purpose.
3. **Access to off-Street Parking**
 - a. Rear parking areas shall be accessed from side streets or rear alleys where such access is available.
 - b. Driveway access from the Highway 12 corridor shall only be permitted if no other alternative exists. No more than one driveway on Highway 12 shall be permitted per parcel.
 - c. Driveways along Highway 12 shall be shared between adjoining lots whenever feasible.
 - d. Driveways and curb-cuts shall be no wider than the minimum necessary.
4. **Central Parking Areas.** Central parking areas that serve a number of buildings and uses are encouraged to allow people to park once and shop at multiple locations



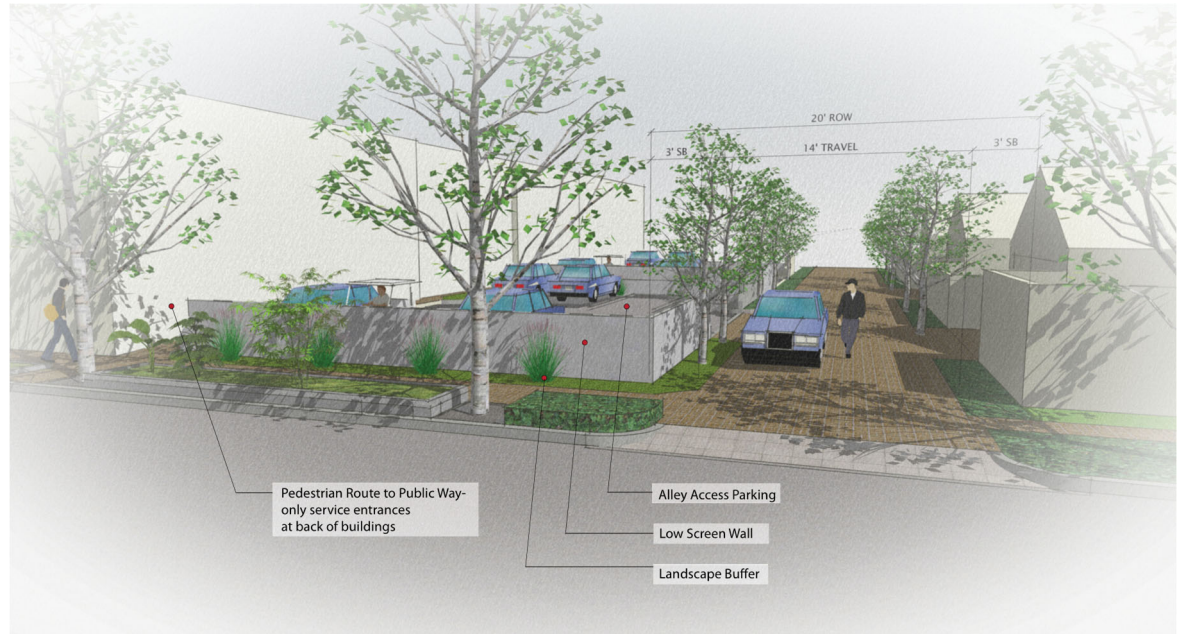
Podium parking consists of at least one floor of at or below ground level with business or residential uses on the upper floors. Image source: City of Novato General Plan Whitepaper.

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within the Springs. Parking areas must include spaces for motorcycles and carpools, parking for bicycles, and charging area for electric vehicles.

5. Landscaping

- a. Interior landscaping of parking areas must be designed to soften the visual impact of parked cars and offer shade. Areas for stormwater retention should also be provided.
- b. Perimeter landscaping should be at least two feet deep.
- c. Parking lots should provide planting islands between parking spaces and at the end of parking rows to break up large expanses of pavement. Planting islands should be provided every eight spaces.
- d. Canopy trees should be located throughout the parking lot, approximately every 30 feet. Canopy trees should have a minimum 30-foot canopy potential.
- e. Landscaping within parking areas should be protected from encroaching vehicles by concrete curbing or raised planters. Wheel stops are not permitted.
- f. Permeable paving and generous landscaping are strongly encouraged.



Example of alley access parking.

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6. Screening of Parking Areas

- a. A three-foot high fence, wall, or other visual barrier (raised planter, benches, etc.) must be provided in combination with landscaping to screen and separate parked vehicles from the street.
- b. Walls and fences must include architectural detailing designed to complement the development and greater Springs community.
- c. The buffer should be designed to provide for stormwater retention.

D. Service Areas

Objective: Ensure that equipment and utility areas do not detract from the visual appearance of the site.

- 1. **General Requirements.** Equipment, utilities, trash collection, etc. shall be, to the extent feasible:
 - a. Located to the rear of buildings
 - b. Screened from public view by wall or enclosure
 - c. Consolidated in one area
 - d. Incorporated into the design of the building
- 2. **Screening**
 - a. Walls and enclosures must be architecturally compatible in design, color, and material with the primary building and must be carefully integrated into the overall project design.
 - b. Walls and enclosures must be constructed of durable materials and designed to adequately conceal its contents.
 - c. Walls and enclosures must be integrated into the overall site design to provide for ease of access and to minimize visual impacts.

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- d. Landscaping should be provided to enhance the appearance of walls and enclosures.
 - e. Trash enclosures must be covered and provided with adequate access for trash collection trucks.
 - f. Project plans must include the location, design, and materials of screening elements for all service equipment and utility areas.
 - g. Cyclone fencing shall not be used for screening.
3. **Roof-top equipment.** Roof-top equipment shall be concealed from public view. Architectural elements used to screen equipment shall be well integrated with the building’s architecture and designed to present a unified appearance.
 4. **Electrical Equipment.** Equipment such as transformers, shall be located to minimize its visual impact and be screened from view whenever possible.
 5. **Loading Area.** Uses requiring the loading and unloading of merchandise should provide adequate space on site for this purpose. Loading docks should be located at the rear of buildings.

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F. Environmental Measures

The following measures apply to development projects, including all ministerial and all discretionary projects, in the Springs.

Air Quality

Measure Air-A: Future project proponent(s) of development, infrastructure, and other land-disturbing projects shall adhere to the *Basic Construction Mitigation Measures* established by the Bay Area Air Quality Management (BAAQMD) CEQA Guidelines 2017, as amended.

Measure Air-B: Prior to the approval of entitlements or permitting operation of project with sensitive receptors (e.g. residential uses, new or expanded daycares, schools, parks, nursing homes, or medical facilities) that are located within a TAC source, including 1,000 feet of Highway 12 or 300 feet of a gas station, the project applicant(s) shall incorporate appropriate measures into the individual project design in order to reduce the potential health risk due to exposure to toxic air contaminants. The project applicant shall choose one of the following methods:

1. The project applicant shall retain a qualified air quality consultant to prepare a Health Risk Assessment (HRA) in accordance with California Air Resources Board (CARB) and Office of Environmental Health and Hazard Assessment requirements to determine the health risk of exposure of project residents/occupants/users to air pollutants. The HRA shall be submitted to the County for review and approval. If the HRA concludes that the health risk is at or below acceptable levels, then health risk reduction measures are not required. If the HRA concludes that the health risk exceeds acceptable levels, health risk reduction measures shall be identified to reduce the health risk to acceptable levels. Identified risk reduction measures shall be submitted to the County for review and approval and be included on the project drawings submitted for the construction-related permit or on other documentation submitted to the County;

OR

2. The project applicant shall incorporate the following health risk reduction measures into the project. These features shall be submitted to the County for review and approval and be included on the project drawings submitted for the construction-related permit or on other documentation submitted to the County:
 - Installation of air filtration to reduce cancer risks and Particulate Matter (PM) exposure for residents and other sensitive populations in the project that are in close proximity to sources of air pollution. Air filter devices shall be rated MERV-13 or

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higher. As part of implementing this measure, an ongoing maintenance plan for the building’s HVAC air filtration system shall be required.

- The project shall be designed to locate sensitive receptors as far away as feasible from the source(s) of air pollution. Operable windows, balconies, and building air intakes shall be located as far away from the TAC sources as feasible. If near a distribution center, residents shall be located as far away as feasible from a loading dock or where trucks concentrate to deliver goods.
- Sensitive receptors shall be located on the upper floors of buildings or, if located on the ground floor, shall be located toward the edge of the property boundary that is farthest from the TAC source.
- Planting trees and/or vegetation between sensitive receptors and pollution source, if feasible. Trees that are best suited to trapping PM shall be planted, including one or more of the following: Pine (*Pinus nigra* var. *maritima*), Cypress (*X Cupressocyparis leylandii*), Hybrid poplar (*Populus deltoids X trichocarpa*), and Redwood (*Sequoia sempervirens*).
- Existing and new diesel generators shall meet CARB’s Tier 4 emission standards, if feasible.

The project applicant(s) shall maintain, repair, and/or replace installed health risk reduction measures, including but not limited to the HVAC system (if applicable), on an ongoing and as-needed basis. Prior to occupancy, the project applicant(s) shall prepare and then distribute to the building manager/operator an operation and maintenance manual for the HVAC system and filter including the maintenance and replacement schedule for the filter.

Measure Air-C: Prior to approval of entitlements or permitting operation of any new or modified commercial building/use that would emit toxic air contaminants (such large-scale auto repairs service centers, gas stations or dry cleaning operations), prioritization screening shall be performed in accordance with the Air Toxics "Hot Spots" Program, Facility Prioritization Guidelines (July 1990) and the Air Toxics "Hot Spots" Information and Assessment Act. The prioritization screening shall be performed in accordance with the California Air Pollution Control Officers Association Air Toxic “Hot Spots” Program guidance. The prioritization screening shall also be conducted consistent with the guidance provided by the Bay Area Air Quality Management District’s (Air District) latest guidance, which will be responsible for determining which facilities must perform a health risk assessment.

If a health risk assessment is warranted for a facility based on its prioritization score, the project applicant shall retain a qualified air quality consultant to prepare an assessment the facilities for the potential to expose the public to toxic air contaminants in excess of the applicable thresholds (utilizing an air dispersion modelling program such as AERMOD). Facilities that exceed the applicable threshold(s) have the potential to expose the public to toxic air contaminants levels that would be considered significant. Facilities that exceed the applicable threshold(s) shall incorporate mitigation to reduce the risks from emission of toxic air contaminants to an acceptable level (i.e., to a level

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that does not exceed the applicable threshold[s]). Potential mitigation includes: reducing the size of the facility area; rearranging the site to reduce the potential for impacts on the nearest sensitive receptors; and utilizing products that reduce the level of toxic air contaminants, or removal of such products from the operational phase of the project.

Biological Resources

Measure Bio-A: On parcels adjacent to the Agua Caliente Creek Corridor and the Pequeno Creek Corridor, future projects subject to a grading permit shall retain a biologist to perform special-status plant surveys. The surveys shall be performed during the floristic season. If any special-status plants are found during the surveys, the project proponent(s) shall contact Permit Sonoma to obtain the appropriate avoidance and minimization measures and shall implement the measures, including throughout project design, construction, and operation, as appropriate. Projects where avoidance or minimization is not feasible are subject to a use permit.

Measure Bio-B: Future projects that require a grading permit within the Plan area shall implement the following measures to avoid or minimize impacts on special-status amphibian and reptile species:

- Preconstruction surveys for California giant salamander, California red-legged frog, foothill yellow-legged frog, red-bellied newt, and western pond turtle shall be conducted by a qualified biologist in all areas of suitable habitat (e.g., the Agua Caliente Creek Corridor, the Pequeno Creek Corridor, and the upland areas associated with either creek) within 500 feet of project disturbance. Surveys shall be conducted within 24 hours before project disturbance.
- If any of these species are found during preconstruction surveys, activities within 200 feet of the find shall cease until appropriate corrective measures have been completed or it is determined by the qualified biologist and County staff, in coordination with U.S. Fish and Wildlife Service (USFWS) and California Department of Fish and Wildlife (CDFW), that the species will not be harmed by the continuation of activities. Any sightings or incidental take shall be reported to USFWS and CDFW immediately.
- Construction personnel performing activities within aquatic habitats and adjacent uplands to be disturbed by project activities shall receive worker environmental awareness training from a qualified biologist to instruct workers to recognize the species, their habitats, and measures being implemented for its protection. Verification shall be provided to County confirming that workers have received environmental awareness training.



Western pond turtle. Image source: California Department of Fish and Wildlife.

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- Construction personnel shall observe a 15-miles-per-hour speed limit on unpaved roads.

Measure Bio-C: Future development projects within 100-feet of Agua Caliente Creek or Pequeno Creek shall be subject to the provisions of the Riparian Corridor Combining Zone.

Measure Bio-D: Future development projects within the Plan area shall implement the following measures to avoid or minimize impacts to special-status birds that may occur on the site:

- Preconstruction surveys for active nests of bank swallow, black swift, burrowing owl, California horned lark, ferruginous hawk, grasshopper sparrow, great blue heron, great egret, northern harrier, Swainson’s hawk, western yellow-billed cuckoo, and white-tailed kite shall be conducted by a qualified biologist in all areas of suitable habitat (e.g., open grassland or field areas, larger trees throughout the Plan area, Agua Caliente Creek Corridor, Pequeno Creek Corridor, and the upland areas associated with either creek) within 500 feet of project disturbance. Surveys shall be conducted within 14 days before commencement of any construction activities that occur during the respective nesting seasons in a given area.
- If any active nests, or behaviors indicating that active nests are present, are observed, appropriate buffers around the nest sites shall be determined by a qualified biologist to avoid nest failure resulting from project activities. The size of the buffer shall depend on the species, nest location, nest stage, and specific construction activities to be performed while the nest is active. The buffers may be adjusted if a qualified biologist determines it would not be likely to adversely affect the nest. If buffers are adjusted, monitoring will be conducted to confirm that project activity is not resulting in detectable adverse effects on nesting birds or their young. No project activity shall commence within the buffer areas until a qualified biologist has determined that the young have fledged or the nest site is otherwise no longer in use.

Measure Bio-E: Future project proponent(s) of development projects within the Plan area shall implement the following measures to avoid or minimize impacts on special-status bats:

- If a project will disturb roosting areas (i.e. buildings, trees, shrubs, bridges, etc.) during the bat pupping season (April 1 through July 31), surveys for active maternity roosts shall be conducted by a qualified biologist. The surveys shall be conducted from dusk until dark.
- If a special-status bat maternity roost is located, appropriate buffers around the roost sites shall be determined by a qualified biologist and implemented to avoid destruction or abandonment of the roost resulting from habitat removal or other project activities. The size of the buffer shall depend on the species, roost location, and specific construction activities to be performed in the vicinity. No project activity shall commence within the buffer areas until the end of the pupping season (August 1) or until

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a qualified biologist confirms the maternity roost is no longer active.

Cultural Resources

Measure Cult-A: All discretionary projects shall be referred to the Information Center for a preliminary review of the project area for previous studies, resources and overall sensitivity. If a known historical, cultural, or archaeological resource is identified in the project area, the area has been determined to be sensitive for these resources, or the project area has not been surveyed recently, then a cultural resource survey, performed by an appropriately qualified professional, shall be required prior to approval or issuance of any discretionary project.

Measure Cult-B: If cultural resources are identified within the project area, they shall be recorded and evaluated by an appropriately qualified professional. For tribal cultural resources identified within the project area, the County of Sonoma shall consult with the culturally affiliated Tribe(s). The County of Sonoma shall take the appropriate avoidance, minimization, and mitigation measures necessary to reduce adverse impacts to the resources to a less than significant level, such as preservation in place, capping the site, documentation, data recovery, awareness training for construction workers, or conservation.

Measure Cult-C: If cultural resources are identified within the project area, they shall be recorded and evaluated by an appropriately qualified professional. For tribal cultural resources identified within the project area, the County of Sonoma shall consult with the culturally affiliated Tribe(s). The County of Sonoma shall take the appropriate avoidance, minimization, and mitigation measures necessary to reduce adverse impacts to the resources to a less than significant level, such as preservation in place, capping the site, documentation, data recovery, awareness training for construction workers, or conservation.

Measure Cult-D: If any subsurface historic remains, prehistoric or historic artifacts, other indications of archaeological resources, or cultural and/or tribal resources are found during grading and construction activities, all work within 100 feet of the find shall cease, the County of Sonoma shall be notified, and the applicant shall retain an appropriately qualified archaeologist to evaluate the find(s). If tribal resources are found during grading and construction activities, the applicant shall notify the Native American Heritage Commission and any Tribes that have been identified as having cultural ties and affiliation with the geographic area in which the archaeological resources were discovered. The County of Sonoma, the archaeologist, and/or the Tribe(s), shall determine the eligibility of the feature(s). If the features are determined ineligible, additional work shall not be required. However, if found eligible, further avoidance, minimization, and mitigation would be necessary, pursuant to Measure Cult-C above.

Measure Cult-E: If any paleontological resources are found during grading and construction activities, all work within 100 feet of the find shall cease, the County of Sonoma shall be notified, and the applicant shall retain an appropriately qualified paleontologist to determine

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the significance of the discovery. The paleontologist shall define the physical extent and nature of the deposit. The County of Sonoma shall take the appropriate avoidance, minimization, and mitigation measures necessary to reduce adverse impacts to the resources to a less than significant level.

Measure Cult-F: Pursuant to CEQA Guidelines Section 15064.5(e) if human remains are discovered during the course of construction, work shall be halted at the site and any nearby area reasonably suspected to overlie adjacent human remains until the Sonoma County Coroner has been informed and has determined that no investigation of the cause of death is required. If the Sonoma County Coroner determines that the remains are of Native American origin, the Coroner shall contact the Native American Heritage Committee (NAHC) within 24 hours of the discovery. The NAHC shall identify the person or persons it believes to be the most likely descended from the deceased Native American. The most likely descendent may make recommendations to the landowner or the person responsible for the excavation work, for means of treating or disposing of, with appropriate dignity, the human remains and any associated grave goods as provided in Public Resources Code section 5097.98, which may include obtaining a qualified archaeologist or team of archaeologists to properly excavate the human remains. If the NAHC is unable to identify a most likely descendent or the most likely descendent failed to make a recommendation within 24 hours after being notified by the commission, if the descendant identified fails to make a recommendation, or if the landowner or his authorized representative rejects the recommendation of the descendant, and the mediation by the Native American Heritage Commission fails to provide measures acceptable to the landowner, then the landowner or his authorized representative shall rebury the Native American human remains and associated grave goods with appropriate dignity on the property in a location not subject to further subsurface disturbance.

Noise

Measure Noise-A: Development projects within the Specific Plan area shall implement and comply with the following measures related to noise:

1. Interior noise levels shall be in compliance with the County interior noise level standard. When exterior noise levels exceed 60 dB Ldn, it is likely that the interior noise levels would exceed 45 dB Ldn, with windows in the open position. Therefore, air conditioning shall be provided to allow residents to close windows and doors to maintain the proper acoustical isolation.
2. Proper residential outdoor activity area setbacks and/or site design shall be provided for projects located along the following roadway segments:

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- Highway 12: Maintain a setback or use site planning to reduce traffic noise levels to 65 dB LDN or lower at outdoor activity areas. A noise study shall be conducted for those residential uses within 240-feet of the Highway 12 centerline to ensure that the County’s exterior and interior noise standards are met.
 - Donald Street east of Robinson: Maintain a setback or use site planning to reduce traffic noise levels to 65 dB LDN or lower at outdoor activity areas. A noise study shall be conducted for those residential uses within 54-feet from the Donald Street centerline.
 - Verano Avenue from Arnold to Highway 12: Maintain a setback or use site planning to reduce traffic noise levels to 65 dB LDN or lower at outdoor activity areas. A noise study shall be conducted for those residential uses within 65-feet from the Verano Avenue centerline.
3. A noise study shall be conducted in accordance with the Guidelines for the Preparation of Noise Analysis for proposed residential development and other noise-sensitive uses at the time site plans are developed or specific uses are proposed in the above-listed areas. The noise study shall include, but not be limited to the following components:
- a. Executive Summary: An overview of the scope of the Noise Analysis, primary components, the current land use, General Plan and zoning designation of the project site and in the project vicinity, and findings;
 - b. Introduction: Describe the purpose of the analysis, need for the project, and need for the study;
 - c. Fundamentals of Noise and Ground Vibration: Discuss physical principals of noise and ground vibration, how the human ear perceives different types of noise, and a description of the types of noise the project could generate, including single event noise and sleep disturbance;
 - d. Project Description: A description of the proposed development, including a description of each of its components, the size and intensity of each of the components, a site plan, cross-sections illustrating elevation and relationship to property lines and noise-generating uses, and the proximity of each project component to a nearby noise source;
 - e. Study Area: Identify the study area, including any potentially impacted existing and future noise sensitive land uses or areas,
 - f. Methodologies and Assumptions: Describe methodologies and assumptions used to determine existing and future noise levels and noise-related impacts;

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- g. Noise Measurement Sites: Select noise measurement sites consistent with specific criteria and discuss criteria for selection and adjustments made;
 - h. Measuring Times, Durations, Repetition, and Procedures: Describe approach to noise measurements and address specific measurement requirements;
 - i. Thresholds: Identify applicable noise thresholds;
 - j. Existing Noise Environment: Discussion of ambient noise levels, including traffic, railroad, other nearby noise generating uses, or airport noise (if applicable);
 - k. Future Noise Levels: Discussion of anticipated future noise levels including traffic, railroad, other nearby noise generating uses or factors, including parking lot noise, emergency generators, sleep disturbance, low frequency noise and vibration, temporary construction noise, periodic noise events, or airport noise;
 - l. Future Cumulative Noise Levels: discuss any potential cumulative noise levels assuming development of the project, and past, current, and reasonably foreseeable development, including if the project has any incremental contribution to noise levels that are cumulatively considerable;
 - m. CEQA Initial Study Checklist: Specifically address each of the questions included in the CEQA Initial Study Checklist;
 - n. Noise Impacts: Analyze the siting of the facilities, impact of the proposed project and siting and nearby noise-sensitive land uses, discuss proposed noise-generating features, including but not-limited to such as loading docks, mechanical equipment, and outdoor entertainment areas, identify impact of noise-generating features on surrounding land uses, calculate noise levels at property lines and sensitive receptors, and identify potentially significant adverse impacts prior to implementation of mitigation measures; and
4. Mitigation Measures: Identify mitigations considered (sound walls, building placement/orientation, hours of operation, equipment modification, etc.), associated noise reductions, areas where abatement/mitigation is not feasible, and justification for infeasibility determinations, and any recommendations.

The noise analysis shall demonstrate that the measures incorporated into the project, such as site design measures including setbacks, shielded outdoor activity areas for residential uses, orientation of buildings, and noise barriers, are adequate to meet the County standard for outdoor activity area noise exposure. The noise analysis shall demonstrate that measures incorporated into

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the project, including site design measures as well as building methods, such as windows with adequate sound transmission class (STC) ratings, location of mechanical ventilation penetrations, and routing of duct work, are adequate to meet the County standard for indoor noise levels.

Measure Noise-B: Construction activities shall include the following best management practices for control of construction noise:

- Construction period shall be less than 12 months or shall be phased and analyzed in accordance with the Guidelines for the Preparation of Noise Analysis, which include but are not limited to the following requirements:
 - Executive Summary: An overview of the scope of the Noise Analysis, primary components, the current land use, General Plan and zoning designation of the project site and in the project vicinity, and findings,
 - Introduction: Describe the purpose of the analysis, need for the project, and need for the study,
 - Fundamentals of Noise and Ground Vibration: Discuss physical principals of noise and ground vibration, how the human ear perceives different types of noise, and a description of the types of noise the project could generate, including single event noise and sleep disturbance,
 - Project Description: A description of the proposed development, including a description of each of its components, the size and intensity of each of the components, a site plan, cross-sections illustrating elevation and relationship to property lines and noise-generating uses, and the proximity of each project component to a nearby noise source,
 - Study Area: Identify the study area, including any potentially impacted existing and future noise sensitive land uses or areas,
 - Methodologies and Assumptions: Describe methodologies and assumptions used to determine existing and future noise levels and noise-related impacts,
 - Noise Measurement Sites: Select noise measurement sites consistent with specific criteria and discuss criteria for selection and adjustments made,
 - Measuring Times, Durations, Repetition, and Procedures: Describe approach to noise measurements and address specific measurement requirements,
 - Thresholds: Identify applicable noise thresholds,
 - Existing Noise Environment: Discussion of ambient noise levels, including traffic, railroad, other nearby noise generating uses, or airport noise (if applicable),
 - Future Noise Levels: Discussion of anticipated future noise levels including traffic, railroad, other nearby noise generating uses or factors, including parking lot noise, emergency generators, sleep disturbance, low frequency noise and vibration, temporary construction noise, periodic noise events, or airport noise,

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- Future Cumulative Noise Levels: discuss any potential cumulative noise levels assuming development of the project, and past, current, and reasonably foreseeable development, including if the project has any incremental contribution to noise levels that are cumulatively considerable,
- CEQA Initial Study Checklist: Specifically address each of the questions included in the CEQA Initial Study Checklist,
- Noise Impacts: Analyze the siting of the facilities, impact of the proposed project and siting and nearby noise-sensitive land uses, discuss proposed noise-generating features, identify impact of noise-generating features on surrounding land uses, calculate noise levels at property lines and sensitive receptors, and identify potentially significant adverse impacts prior to implementation of mitigation measures,
- Mitigation Measures: Identify mitigations considered (sound walls, building placement/orientation, hours of operation, equipment modification, etc.), associated noise reductions, areas where abatement/mitigation is not feasible, and justification for infeasibility determinations, and any recommendations to meet applicable County noise standards.
- Construction staging areas and construction activity areas shall be sited to minimize noise exposure to nearby residents and noise-sensitive uses.
- Noise-generating construction activities, including truck traffic coming to and from the construction site for any purpose, shall be limited to between the hours of 7:00 am and 7:00 pm on weekdays, and between 7:00 am and 5:00 pm on weekends.
- Limiting work to non-motorized equipment on Sundays and Holidays.
- All equipment driven by internal combustion engines shall be equipped with street-legal mufflers, which are in good condition and appropriate for the equipment.
- The construction contractor shall utilize “quiet” models of air compressors and other stationary noise sources where technology exists.
- Using sound blankets for loud operations such as pile driving, air compressors or other mechanical equipment, and consider pre-drilling holes prior to pile driving
- At all times during project grading and construction, stationary noise-generating equipment shall be located as far as practicable from sensitive receptors and placed so that emitted noise is directed away from residences.
- Unnecessary idling of internal combustion engines shall be prohibited.
- Construction staging areas shall be established at locations that will create the greatest distance between the construction-related noise sources and noise-sensitive receptors nearest the project site during all project construction activities, to the extent feasible.
- Neighbors located adjacent to the construction site shall be notified of the construction schedule in writing and provided with the contact information of the construction project manager or noise coordinator.

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- The construction contractor shall designate a “noise disturbance coordinator” who will be responsible for responding to any local complaints about construction noise. The disturbance coordinator shall be responsible for determining the cause of the noise complaint (e.g., starting too early, poor muffler, etc.) and instituting reasonable measures as warranted to correct the problem. A telephone number for the disturbance coordinator shall be conspicuously posted at the construction site.

Measure Noise-C: The County shall resurface the section of Robinson Road between Donald Street and East Verano Street and the section of Donald Street east of Robinson Road with either rubberized asphalt or open gap asphalt. This improvement shall be included in the Specific Plan fee program and Specific Plan projects shall contribute their fair-share of the cost to improve and maintain the rubberized or open gap asphalt.

Tribal Cultural Resources

Measure TCR-A: Tribal Cultural Resources Coordination and Consultation. If during the implementation of Mitigation Measure CUL-A, archival research results in the identification of an association between a historical built-environment resource and a local California Native American tribe, the qualified architectural historian or historian shall confer with the local California Native American tribe(s) on the implementation of Mitigation Measure CUL-B. Throughout the implementation of Mitigation Measures CUL-C through CUL-I, the qualified archaeologist retained to implement the measures shall confer with local California Native American tribe(s) on the identification and treatment of tribal cultural resources and/or resources of Native American origin not yet determined to be tribal cultural resources through AB 52 consultation. If, during the implementation of Mitigation Measures CUL-C through CUL-I, a resource of Native American origin is identified, the County shall be notified immediately in order to open consultation with the appropriate local California Native American tribe(s) to discuss whether the resource meets the definition of a tribal cultural resource as defined in AB 52.

Measure TCR-B: Avoidance of Tribal Cultural Resources. When feasible, development facilitated by the project shall be designed to avoid known tribal cultural resources. Any tribal cultural resource within 60 feet of planned construction activities shall be fenced off to ensure avoidance. The feasibility of avoidance of tribal cultural resources shall be determined by the County and applicant in consultation with local California Native American tribe(s).

Measure TCR-C: Tribal Cultural Resources Plan. A Tribal Cultural Resources Plan shall be required for Potential Sites identified as potentially sensitive for tribal cultural resources during consultation with local California Native American tribe(s) during the implementation of TCR-A and/or by the qualified archaeologist during the implementation of CUL-C through CUL-I. Prior to any development facilitated by the project that would include ground disturbance, the project applicant or its consultant, shall prepare a tribal cultural resources treatment plan to be implemented in the event an unanticipated archaeological resource that may be considered a tribal cultural resource is identified during

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construction. The plan shall include any necessary monitoring requirements, suspension of all earth-disturbing work in the vicinity of the find, avoidance of the resource or, if avoidance of the resource is infeasible, the plan shall outline the appropriate treatment of the resource in coordination with the local Native Americans and, if applicable, a qualified archaeologist. Examples of appropriate treatment for tribal cultural resources include, but are not limited to, protecting the cultural character and integrity of the resource, protecting traditional use of the resource, protecting the confidentiality of the resource, or heritage recovery. As appropriate, the tribal cultural resources treatment plan may be combined with any Extended Phase I, Phase II, and/or Phase III work plans or archaeological monitoring plans prepared for work carried out during the implementation of Mitigation Measures CUL-D, CUL-F, CUL-G, or CUL-H. The plan shall be reviewed and approved by the County and the appropriate local California Native American tribe(s) to confirm compliance with this measure prior to construction.

Measure TCR-D: Native American Monitoring For Potential Sites identified as potentially sensitive for tribal cultural resources through consultation with local California Native American tribe(s) during the implementation of TCR-A and/or identified as sensitive for cultural resources of Native American origin by the qualified archaeologist during the implementation of CUL-C through CUL-I, the project applicant shall retain a locally affiliated Native American monitor to observe all ground disturbance, including archaeological excavation, associated with development facilitated by the project. Monitoring methods and requirements shall be outlined in a tribal cultural resources treatment plan prepared under Mitigation Measure TCR-C. In the event of a discovery of tribal cultural resources, the steps identified in the tribal cultural resources plan prepared under Mitigation Measure TCR-3 shall be implemented.

Measure TCR-E: Sensitive Location of Human Remains. For any development facilitated by the project where human remains are expected to be present based on the results of tribal consultation during the implementation of TCR-A and/or as identified by the qualified archaeologist, the County shall consult with local California Native American tribe(s) on the decision to employ a canine forensics team. If appropriate, the County shall require the use of a canine forensics team to attempt to identify human remains in a noninvasive way (e.g., non- excavation) for the purpose of avoidance, if avoidance is feasible (see Mitigation Measure TCR-B). Any requirements for the use of a canine forensics team shall be documented in the tribal cultural resources treatment plan prepared under Mitigation Measure TCR-C. Pending the results of any canine investigations, the tribal cultural resources treatment plan may require revision or an addendum to reflect additional recommendations or requirements if human remains are present.

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Wildfire

Measure Wildfire-1: In order to reduce fire risk, all projects shall comply with the applicable State fire safety regulations associated with wildland-urban interfaces, fire-safe building standards, and defensible space requirements. All homeowners shall be responsible for clearing out flammable materials, such as brush or vegetation, around their buildings to 100 feet (or the property line) to create a defensible space buffer.

Measure Wildfire-2: New buildings located in the Plan area shall comply with the Wildland-Urban Interface Fire Area Building Standards and Sonoma County Code Chapter 13, which establish minimum standards for materials and provide a reasonable level of exterior wildland fire exposure protection. The standards require the use of ignition resistant materials and design to resist the intrusion of flame or burning embers from a vegetation fire into buildings.



CAL FIRE identifies two defensible space zones to improve a home’s chance of surviving a wildfire. Zone 1 extends 30 feet from buildings and structures and Zone 2 extends 100 feet from buildings and structures. Section 13A-4 of the County Code identifies hazardous vegetation and combustible material abatement requirements for each zone. Image source: www.readyforwildfire.org.

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E. Public Spaces

Objective: Create a network of safe, attractive, and usable public and semi-public outdoor spaces throughout the Springs community to encourage greater pedestrian activity.

1. Outdoor Seating

- a. New development and major renovations shall include well-designed pedestrian amenities, including outdoor seating, whenever feasible to encourage pedestrian activity.
- b. Small plazas, courtyards, and other seating areas should be both visible and accessible from the public sidewalk.
- c. Planters and low walls should be designed to provide informal seating for a project.

2. **Outdoor Furnishings.** The style of outdoor furnishings and light fixtures provided for open air spaces should reflect the design concepts of the site.

3. **Vegetation and Shade.** Outdoor seating areas should be enhanced with shade, vegetation, and pedestrian scaled lighting.

4. **Incorporate Nature.** Development should blend with, preserve, and incorporate existing natural features, including creeks, mature trees, and riparian habitat, into the site design.

5. **Parklets.** Parklets are encouraged in appropriate locations to provide public amenities such as outdoor dining or seating.



Parklets are typically small open spaces that extend sidewalks to provide public gathering spaces along streets.

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IV. SIDEWALK AMENITIES

***Objective 1:** Provide an attractive, inviting, and comfortable pedestrian environment with a variety of amenities to encourage people to walk, shop, and tarry.*

***Objective 2:** Create a sidewalk abounding in flowering plants and greenery to adorn the street frontage, entice pedestrians, and attract shoppers.*

1. **Street Furniture.** Street furniture, such as benches, tables, planters, and trash receptacles, should be provided along the sidewalk adjacent to Highway 12 to create a welcoming streetscape.
2. **Bicycle Racks.** Provide bike racks at convenient locations along the public sidewalk. Creative bike rack designs add personality to the streetscape and are strongly encouraged.
3. **Preserve Walkability.** Street trees, bicycle racks, trash receptacles, and other street furniture must be placed in locations where they will not decrease the walkability of the sidewalk.
4. **Functional Art.** Functional art, such as painted benches and innovative bicycle racks, is strongly encouraged.
5. **Flowering Plants.** Planters, flower pots, hanging baskets, and window boxes brimming with flowering plants should be provided to accentuate entryways and visually enhance storefronts and sidewalks. Trellises with climbing vines are also encouraged.



Street furniture, including benches, potted plants, and trash receptacle, that complements style of lighting fixtures along Highway 12.

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6. Street Trees and Landscaping

- a. Street trees must be provided as part of all new development projects and major renovations, wherever feasible. Where street trees are not feasible, planters or landscaping with shallow root systems shall be used.
- b. A variety of tree species should be used to reduce host-specific diseases. Select trees based on characteristics such as:
 - Appearance
 - Canopy size
 - Flowers and fall color
 - Hardiness
 - Drought tolerance
 - Low maintenance
 - Nonaggressive roots
- c. Trees should be spaced consistent with their canopy size, typically no more than 30 feet on-center.
- d. Street trees should be located no closer than 10 feet to street lights.
- e. Sidewalk tree wells must be at least 3' x 6' in size.
- f. Deep root barriers (at least 24 inches deep) must be used to promote deep rooting and protect adjacent pavement.

Preferred Street Trees

The following tree species are preferred in the Springs due to their low water use, canopy size, and nonaggressive root systems.

Arbutus (*Arbutus Marina*)

Chinese flame tree (*Koelreuteria bipinnata*)

Chinese fringe tree (*Chionanthus retusus*)

Crape myrtle (*Lagerstroemia indica*)

Western Redbud (*Cercis occidentalis*)

Columnar Zelkova (*Zelkove serrata 'Musashino'*)

Goldenrain Tree (*Koelreuteria paniculata*)

Shantung maple (*Acer truncatum*)

Sweet Bay (*Laurus nobilis*)

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- g. Structural soil must be used in tree wells to provide the pore space necessary for healthy tree root growth.
 - h. Street tree wells and planters must be incorporated into the plans for subsurface utilities and equipment on the sidewalk.
 - i. Ornamental tree grates and guards shall be provided to protect trees from damage and extend the walking surface for pedestrians.
7. **Trash Receptacles.** Coordinating trash and recycling receptacles should be provided at strategic locations along the corridor (e.g. near street corners and benches). The containers should be constructed of durable, high quality materials and designed to complement other street furniture, such as benches and planters.
8. **Water Stations.** Water stations should be provided at regular intervals along the corridor and should be designed to complement other street furniture in the vicinity.
9. **Street Lights.**
- a. Pedestrian-scale street lights should be provided at regular intervals along each roadway.
 - b. A traditional luminaire with a decorative post must be used.
 - c. The streetlights must have a full-cutoff optical design.

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V. LANDSCAPING AND FENCES

***Objective:** Ensure that all projects include a generous amount of well-designed landscaping to enhance the site.*

1. **In General.** A generous amount of landscaping should be used to enhance and define public and private spaces.
 - a. Landscaping should consist of a combination of trees, shrubs, and ground cover in a variety of sizes, as appropriate.
 - b. Native plants adopted to the local climate, soil and hydrology should be used generously to reduce the need for irrigation. Nonnative ornamentals may be used as color accents and in planters and pots.
 - c. Landscaping should be extended vertically onto walls through the use of climbing plants, espaliered trees and shrubs, wall and window planters, and roof gardens.
2. **Riparian Areas.** Only native riparian vegetation shall be used in or adjacent to a riparian corridor (see Sonoma County Zoning Code, Article 65).
3. **Safety.** Landscaping should be designed to allow natural surveillance of pedestrian areas.
4. **Fences.** Fences and walls shall not be placed along the Highway 12 non-residential frontage, unless required for the screening of parking areas. Fences, wall, hedges, and similar barriers shall not be more than 3 feet in height and shall be consistent with the requirements of the Sonoma County Zoning Code.

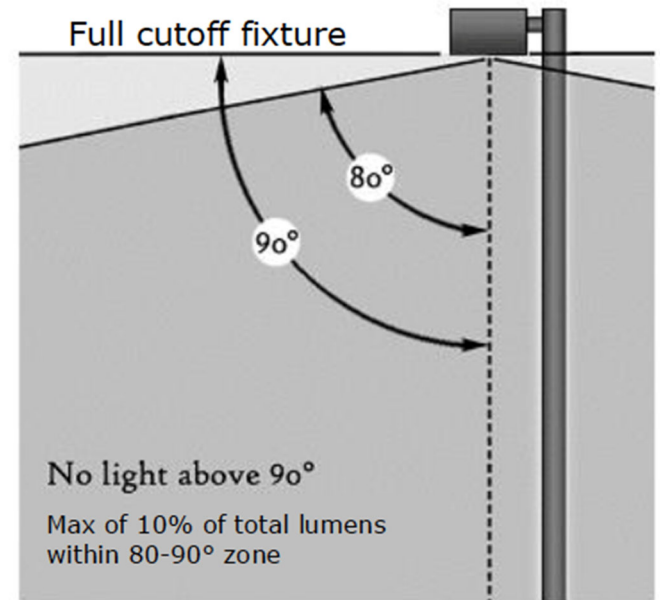
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VI. EXTERIOR LIGHTING

Objective 1: Provide exterior lighting that is designed to enhance the ambiance of the environment and increase pedestrian comfort and safety.

Objective 2: Preserve the dark sky and avoid the spillover of light and glare onto adjacent properties and residences.

1. **Compatible Design.** Light fixtures shall be architecturally compatible with the associated development.
2. **Full Cutoff Light Fixtures.** All exterior lighting shall be designed and positioned to direct light downward and shall not result in glare or spill-over lighting onto any adjacent property or into the night sky. Only full cutoff light fixtures shall be used.
3. **Pedestrian-scale light fixtures.** All exterior lighting shall be pedestrian-scale. Pedestrian-scale light fixtures are lower in height than standard fixtures and spaced closer together.
 - a. Bollard light fixtures should be no more than three feet in height.
 - b. Ornamental post light fixtures should not exceed 12 feet in height.
4. **Accent Lighting.** Subtle, indirect light must be used when illuminating architectural elements, landscape features, building entrances, fountains, and public art. Accent lighting must be cast downward and the light source must be concealed from view.



Full cutoff fixtures have no light escaping above an angle of 90° and reduce the luminous intensity between the 80° and 90° angles to 10% of the light (in lumens) of the fixture. Image credit: Southampton Town Code

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- 5. **Walkways and Outdoor Seating.** All walkways and outdoor seating areas should be illuminated with pedestrian-scale light fixtures to provide for the comfort and safety of pedestrians.
- 6. **Lighting for Signs.** Goose neck lamps are encouraged to illuminate storefront signboards.



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VII. SIGNS

***Objective 1:** Ensure that all signs are well-designed and appropriately sized to complement the building on which it is placed and the community as a whole.*

***Objective 2:** Minimize sign clutter and emphasize pedestrian-scale design.*

General Requirements

1. **Legibility.** All signs shall be clear and legible.
2. **Pedestrian Focus.** The size, design and location of signs should be geared toward pedestrians.
3. **Quality.** Signs shall be designed and constructed with a high level of craftsmanship and durability.
4. **Compatibility.** Signs must complement the architecture, scale, and finish of building. Signs must not clash with the character of the surrounding buildings.
5. **Color.** Sign color must coordinate well with the colors and architecture of the building.
6. **Multi-Tenant Buildings.** A master sign program must be developed for multi-tenant buildings to establish a unifying theme with respect to type, size, location, color, typography, and illumination of all tenant and building signs.



The size and placement of these signs in the pedestrian’s view along the sidewalk provides an example of pedestrian-oriented signage.

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7. **Signs.** With the passage of time, certain signs become so familiar and endearing to a community that they transcend their traditional function of providing identification. Such signs become a distinguishing feature of the streetscape and part of the community memory.
 - a. Retain historic signs whenever possible. Historic signs are encouraged to be retained even after the use has changed and are exempt from the prohibited sign types identified herein. Historic signs include the Boyes Springs Food Center,
 - b. Signage for a new use may be provided in addition to the historic sign associated with the former use.
 - New signage should be sensitive to both the historic sign and the structure.
 - New signs should respect the size, scale, and design of the historic building.
 - Materials used for new signs should be compatible with those of the historic building.

8. Number and Size of Signs
 - a. One identification sign (name and/or logo) is allowed for each building face

9. **Prohibited Signs.** Signs not specifically allowed by these guidelines are prohibited. Some, but not all, of the prohibited signs are listed below.
 - a. Signs that have blinking, flashing, animated or fluctuating light
 - b. Pole signs
 - c. Signs projecting above the roofline or cornice of a building
 - d. Cabinet signs
 - e. Banners, pennants, flags, inflatables, etc., used for advertising



The Boyes Springs Food Center sign contributes to the character of the Springs, as a distinct, recognizable sign reflecting the historical food center building.

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- 10. **Wayfinding Signs.** Thematic wayfinding (directional) signs should be installed along the Highway 12 corridor to direct people to off-street parking and destinations, such as the community plaza and parks.
- 11. **Zoning Code.** All signs must comply with the sign regulations of the County’s Zoning Code (Article 84 – Sign Regulations), except with regard to historic signs and except as otherwise specified herein through specific provisions for attached wall signs, awning signs, hanging signs, and windows signs.

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A. Attached Wall Signs

Wall signs consist of individual, three-dimensional letters or symbols and affixed to the exterior wall of a building.

Location

1. Wall signs must be positioned within an architectural feature, such as the signboard or fascia band located above the storefront, when the building has been provided with such a feature.
 - a. Wall mounted signs placed within fascias, signboards, or other architectural elements, must be sized to fit within these features and must not extend beyond them.
 - b. Where no signboard or fascia exists, wall signs should be mounted in locations that respect the design of the building. Signs should generally be placed within a band or blank area just above the ground level display windows.
2. Visual continuity among storefronts should be established by placing new signs consistent with appropriately located signs on adjacent buildings.
3. New buildings should include an architectural element or location specifically designed to accommodate signage. The size, location, and design of the sign placement area should be consistent with the architecture and size of the building as well as the area’s historical and architectural context.
4. Wall signs must not be placed over building details, cornices, moldings, windows, or other design features of the building.
5. Sign must not be located more than 20 feet above the sidewalk.
6. Signs must never extend above the roofline of a building.

Design

1. **Legible Text.** Signs should be simple, clear and legible.
2. **Incorporate into Building Design.** Signs must integrate well with the design of the building, much the same as architectural elements. The sign should be consistent with the building’s architecture, scale, materials, and color.

4-50	Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-454	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61
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3. **Descriptive Graphics.** Signs that reflect the type of business through design, shape, or graphic form are encouraged.
4. **No Cabinet Signs.** Cabinet signs are prohibited.
5. **Size/Scale**
 - a. The size and scale of a sign must be appropriate for the building on which it is placed and compatible with the area in which it is located.
 - b. The maximum size of an attached wall sign is 32 square feet.
 - c. Individual letters should be no more than 18 inches high.
 - d. Attached signs must not project more than 10 inches from the building wall.
6. **Number.** Only one attached wall sign shall be permitted on each side of the building that fronts onto a street or public space.
7. **Colors & Materials.** Sign materials and colors must be consistent or compatible with the architecture, color, and materials of the building on which the sign is placed.
8. **Illumination**
 - a. The type of illumination used should be consistent with the architecture of the building and the character of the other buildings along that portion of the street.
 - b. External illumination should be used for signs in areas with historical character. Gooseneck lamps are recommended for the Boyes Boulevard commercial core.
 - c. Individual channel letters may be internally illuminated. Back-lit, halo-lit or reverse channel letters with halo illumination should be used.
 - d. Cabinet signs shall not be used.

Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-45	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61	4-51
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B. Hanging Signs

A hanging sign is typically a small, double-sided sign that hangs from a brace mounted to the wall of the building. A hanging sign is generally intended to be read by pedestrians walking along a sidewalk and by motorists in slow-moving vehicles.

Location

1. **Near Entrance.** The hanging sign should be mounted perpendicular to the exterior wall, near the entrance to the building and should be readily visible to pedestrians.
2. **Distance from Wall.** The hanging sign should not project more than four feet from the face of the building.
3. **Distance above Sidewalk**
 - a. The sign should provide a minimum of eight feet of clearance between the sidewalk and the bottom of the sign.
 - Clearance may be reduced to seven feet when the hanging sign is mounted under a canopy.
 - Clearance may be reduced to five feet when the sign is located above a planter or other landscape or architectural feature which prevents pedestrians from walking under the sign.
 - b. The hanging sign must be mounted below the ground floor’s ceiling line or no higher than 12 feet above the sidewalk, whichever is less.



This hanging sign incorporates descriptive graphics that illustrate the type of business, a gelato shop, through depicting gelato scoops atop a cone.

4-52	Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-454	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61
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Design

1. **Graphics**
 - a. Hanging signs may include graphics, logos, and symbols.
 - b. The use of raised or recessed lettering, symbols, and borders is encouraged.
 - c. Signs that reflect the type of business through design, shape, or graphics are encouraged.
2. **Materials.** Signs should be constructed of wood or metal, with carved or applied lettering. Other materials that are architecturally compatible with the building may also be used.
3. **Brackets**
 - a. Mounting brackets should be compatible with the design and scale of the sign and the architecture of the building.
 - b. Decorative iron and wood brackets are encouraged.
4. **Illumination**
 - a. Hanging signs may be illuminated with an external light source.
 - b. The external light source must be designed to avoid glare.
 - c. Internal illumination shall not be used.
5. **Size.** The area of a hanging sign shall not exceed five (5) square feet.
6. **Number.** No more than one hanging sign shall be allowed per business.

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C. *Window Signs*

Window signs consist of permanent letters or logos either painted, etched or adhered to the inside surface of a display window. These signs are intended to be viewed by pedestrians walking along the sidewalk.

1. **Size.** Window signs must not cover more than 25 percent of the window.
2. **Number.** Only one window sign shall be permitted per business on any one street frontage.
3. **Letters and Symbols**
 - a. Text shall be limited to the store name and specific product or service provided.
 - b. Graphic logos and images may be used along with text.
 - c. Letters should not exceed eight inches in height.
 - d. Clear space should be incorporated into the sign graphics and between letters to continue to allow visibility into the interior of the building.
4. **Materials.** High quality materials and application methods must be used, including paint, etching or vinyl film applied to the inside of the window. White paint and gold-leaf are recommended for painted window signs. Paper signs shall not be permitted.
5. **Transparency.** A window sign must not significantly reduce visibility into the business from the sidewalk.

D. *Awning Signs*

An awning sign may be used in lieu of window sign.

1. Lettering shall be allowed on awning valances only.
2. The sign shall be limited to no more than 60 percent of the area of the valance.
3. Letters shall not exceed eight inches in height and shall be contained in a single line.

4-54	Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-454	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61
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- 4. Lettering or logos shall not be allowed on the canopy of the awning.
- 5. Awning signs shall not be backlit or illuminated.

Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-45	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61	4-55
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VIII. SPRINGS PLAZA

***Objective:** Create an attractive, vibrant public plaza to serve as a center for community life, civic engagement, informal gatherings and community-wide events.*

1. **Location.** The preferred location for the plaza is shown by Figure 4.
2. **Design for Flexibility.** The plaza should be designed to accommodate a wide range of social functions. Avoid fragmentation of the plaza to provide more flexibility.
3. **Amenities.** The plaza should be enhanced with amenities such as landscaping, shade, water features, public art, trellises, a water station, and textured or colored paving. The plaza should be equipped with power outlets to facilitate public events and performances.
4. **Seating.** The plaza should be provided with a variety of seating options, such as benches and portable chairs, as well as informal seating (e.g. planters and low walls).
5. **Lighting.** The plaza should be well-lit to allow for evening use and promote safety. Pedestrian-scale lighting, no more than 12 feet high, should be used.
6. **Vehicle Parking.** Parking shall be prohibited on the plaza, except for vendor or other vehicles that have been given permission by the County to access the plaza for an event, such as a farmers market, concert, art show, or community gathering. Removable bollards shall be placed at vehicle access points to limit vehicle access.
7. **Bicycle Parking.** Bicycle parking facilities shall be provided.
8. **Adjacent Development – Design and Orientation**
 - a. Development located adjacent to the plaza must be designed to have a primary facade oriented toward the plaza. Adjacent development that fronts onto a public street or highway must be designed to face both the plaza and the street.

4-56	Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-45	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61
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- b. Adjacent businesses must be designed to create a friendly and open interface with the plaza. The use of glass doors, operable windows, and walls that slide open, are recommended to allow neighboring buildings to open onto the plaza. Restaurants and other food and beverage businesses are encouraged to spill out onto the plaza.
 - c. New development shall be setback 15 feet from the plaza to provide a walkway along the perimeter of the plaza. The walkway should include amenities, such as benches, tables, potted plants, trash receptacles, etc.
9. **Visibility.** A visual connection to the plaza should be created. Plaza connectors might include:
- a. Columns or signs on each side of Highway 12 and on Boyes Boulevard.
 - b. Special pavement on Highway 12 crosswalks.
 - c. Signage or sidewalk markings at nearby transit stops.

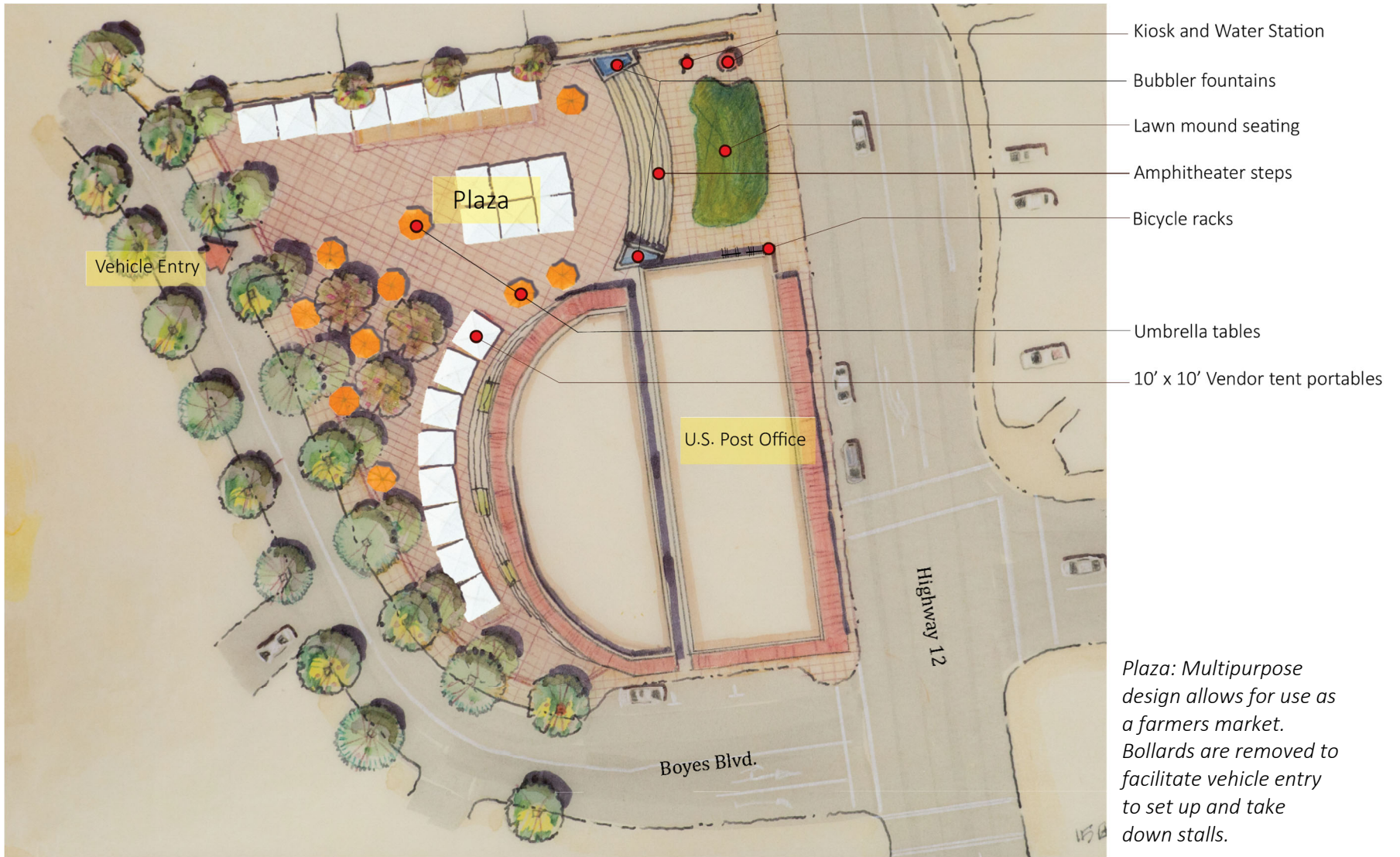
Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-45	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61	4-57
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Plaza: Multipurpose design allows for daily use as a community plaza. Limited vehicle access allows business owner with plaza frontage to utilize parking spaces when no events are planned.

Note: Conceptual rendering only.

4-58	Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-45	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61
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Plaza: Multipurpose design allows for use as a farmers market. Bollards are removed to facilitate vehicle entry to set up and take down stalls.

Note: Conceptual rendering only.

Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-45	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61	4-59
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Note: Conceptual rendering only.

4-60	Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-45	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61
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IX. GATEWAY SIGN

Objective: *Provide prominent, aesthetically pleasing gateway signs to distinguish the Springs corridor from other segments of Highway 12, and to create a sense of arrival and community.*

1. **Location.** Gateway signs should be placed at the northern and southern Highway 12 entrances to the Springs. The signs may not be placed in a location that creates a visual obstruction or hazard.
2. **Design**
 - a. Gateways signs should be designed to complement the iconic architecture of the Springs.
 - b. A prominent monument style sign with stone columns should be used.
 - c. Gateway signs placed at entrances to the Springs shall use similar materials and design in order to provide a cohesive aesthetic.
3. **Materials.** Signs shall be constructed of high quality, durable materials, such as stucco, stone, tile or metal.
4. **Text**
 - a. Sign text should be kept simple and short to ensure readability and quick recognition.
 - b. An easy to read typeface should be used.
 - c. The gateway sign may include the community name, a salutation (e.g. Welcome to the Springs), and short slogan or logo.



The gateway monument at the southern entrance to the Springs provides a basis for the style of gateway signs to be used in the community.

Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-45	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61	4-61
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5. **Illumination**

- a. The gateway sign may be illuminated by either an external light source or by the illumination of individual letters.
- b. External light sources must be concealed from view to prevent glare.
- c. A halo-lit sign with raised metal lettering is recommended.
- d. Internal illumination of individual channel letters against an opaque background may also be used.
- e. Cabinet signs (box signs) are prohibited.

6. **Landscaping.** Ornamental landscaping may be provided at the base of the sign to enhance the gateway. Select plants that will not obstruct the view of the sign.



The illustration to the left is an example of a gateway monument that echoes the shape and characteristics of the gateway arch at the southern entrance to the Springs.



The illustration to the right represents a low gateway monument that echoes the arched shape and stone base of the gateway arch at the southern entrance to the Springs.

4-62	Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-45	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61
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Introduction 4-1	Building Character 4-3	Site Design 4-20	Sidewalk Amenities 4-41	Landscaping and Fences 4-44	Exterior Lighting 4-45	Signs 4-47	Springs Plaza 4-59	Gateway Sign 4-61	4-63
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I. OVERVIEW

This chapter addresses community facilities that will be needed to support implementation of the Specific Plan. The Springs is a developed corridor with established infrastructure systems that support existing residents, businesses, and facilities. This chapter is based on the Utility Infrastructure Report for The Springs Specific Plan prepared by EBA Engineering in September 2019 in coordination with infrastructure providers. Please refer to the Utility Infrastructure Report for a detailed description of infrastructure needs and maps identifying existing water, sewer, and storm drain infrastructure serving The Springs Specific Plan area. This chapter addresses the following topics:

- Water
- Sewer
- Storm drains
- Dry utilities (electricity, natural gas, communications)
- Emergency Services

Overview 5-1	Water 5-2	Sewer 5-4	Storm Drains 5-5	Dry Utilities 5-6	Emergency Services 5-7	Goals and Policies 5-8	5-1
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II. WATER

The Valley of the Moon Water District provides water services to development in the Springs. Figure 13 shows water supply infrastructure in the Plan area.

Existing water mains in the Springs are nearing the end of their useful life and will need to be replaced or upgraded over time to maintain the current level of service. Future development may require new water mains or improvements to the existing water distribution system. The specific improvements required for new construction would be determined for each new project based on the specific development proposal and location. Potential improvements include upgrades to the aging pipe network, booster facilities in areas where pressure is low, and increased water storage in the Donald Street/Verano Avenue neighborhood.

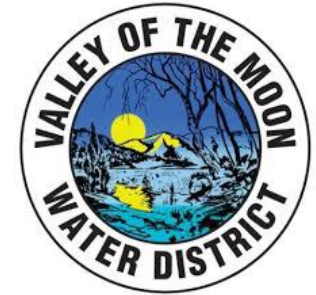


Table 7 summarizes water system improvements that would be considered for future projects.

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Table 7: Water System Improvements

Location	Improvements/Actions
All Areas	Existing mains and facilities are nearing the end of the design lifespan. Assess condition of mains and facilities. Develop a replacement and upgrade strategy.
Balsam Avenue	A booster or other upgrade may be needed to provide adequate pressure for future development. Evaluate on a project-by-project basis.
Bernhard Avenue	A connection between Highway 12 and Balsam Street is recommended for new development to complete grid distribution system.
Donald/Verano Area	Additional storage may be needed to accommodate development at higher densities. Evaluate on a project-by-project basis.
Hooker Avenue	Potential low pressure in this area. A booster or other upgrade may be needed to provide adequate pressure for future development. Evaluate on a project-by-project basis.
Waterman Avenue	Potential low pressure in this area. A booster or other upgrade may be needed to provide adequate pressure for future development. Evaluate on a project-by-project basis.
State Highway 12	A booster or other upgrade may be needed to provide adequate pressure for future development. Evaluate on a project-by-project basis.

III. SEWER

The Sonoma Valley Sanitation District (Sanitation District) provides wastewater collection, treatment, and disposal services within the Plan area.

The existing sewer infrastructure is generally adequate to accommodate development allowed by this Plan. However, many of the existing mains are deteriorating with high levels of infiltration during storm events. Sewer mains must be evaluated and upgraded as necessary to provide adequate service and capacity. The larger tributary area must also be evaluated in several locations. Table 8 summarizes sewer system improvements identified by the Sonoma Valley County Sanitation District to accommodate development in the Plan area.

Table 8: Sewer System Improvements

Project Name (Number)	Description
Depot Road Diversion (P1)	Install 280 linear feet of 10-inch pipe in Depot Road from Mountain Avenue to Malek Road.
Boyes Boulevard Diversion (P3)	Replace 414 lf of 8" pipe with 10" pipe in Sonoma Hwy. and install ~1,330 lf of 10" pipe in Boyes Blvd. from Sonoma Hwy. to Mulberry Ave.
Fairview Lane (P4)	Replace 1,100 lf of 8" pipe with 10" and 12" pipe in Fairview Lane and easement west of Sonoma Hwy.
West Spain Street (P5)	Replace 980 lf of 10" pipe with 15" pipe on north side of West Spain St. from Junipero Serra Dr. to Broadway; abandon 8" sewer on south side of West Spain St., reconnect laterals and install ~70 lf of 8" pipe to divert flow to new sewer.
Junipero Serra Drive (P5 Extension)	Replace 164 lf of 10" pipe with 15" pipe on the southern portion of Junipero Serra Dr.
5th Street West (P14)	Replace 570 lf of 6" pipe with 8" pipe in 5th Street West from W. Spain St. to W. Napa St.

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IV. STORM DRAINS

The Sonoma County Department of Transportation and Public Works provides storm drain services to the Springs area.

The County of Sonoma regulates discharges into the storm drain system. Development projects must comply with the applicable stormwater design strategies and best management practices to control stormwater runoff quality and quantity. Specific requirements are determined for each project on a case-by-case basis. Flood management facilities, including underground drainage infrastructure, connections to existing drainage infrastructure, drainage easements, and creek outlets, would be required in watershed areas 1, 2, 3, 5, 6, 9, 10, 11, 13, 14, 18, and 21. Drainage infrastructure, drainage easements, creek outlets, would be required in watershed areas 7, 8, 15, 16, 17, 19, 23, and 24.

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V. DRY UTILITIES: ELECTRICITY, NATURAL GAS, COMMUNICATIONS

Sonoma Clean Power provides electricity in the Springs via PG&E’s transmission and distribution system. Natural gas service is provided by PG&E. Telecommunications services are provided to the Plan area by Comcast and AT&T. These services will be extended as necessary on a project-by-project basis to adequately serve the Plan area.

5-6	Overview 5-1	Water 5-2	Sewer 5-4	Storm Drains 5-5	Dry Utilities 5-6	Emergency Services 5-7	Goals and Policies 5-8
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VI. EMERGENCY SERVICES

The Sonoma Valley Fire & Rescue Authority (Fire Authority) provides all-risk fire, rescue, and emergency medical services to the Springs area. The Plan area is primarily served by Station 3, located at the intersection of Agua Caliente Road and Highway 12. Station 2, located at 877 Center St, serves the southernmost portion of the Plan area.

Law enforcement services for the Plan area are provided by the Sonoma County Sheriff’s Department. The Sonoma Valley Substation provides patrol services to the entire Sonoma Valley from Pythian Road to San Pablo Bay.



Sonoma Valley Fire and Rescue Authority Station 3, located northwest of the intersection of Agua Caliente Road and Highway 12.

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VII. GOALS AND POLICIES

Goal CF-1: Ensure that community facilities and utilities adequately serve existing and new development in the Springs.



Image of sewer line replacement. Image source: Sonoma Water Capital Improvement Plan 2019-2024

Policy CF-1a Review updates to the Sonoma Valley County Sanitation District sewer plans to ensure that needed that adequate levels of service are maintained under existing and buildout conditions.

Policy CF-1b Review updates to the Valley of the Moon Water District plans to ensure that water lines meet current design standards and adequate levels of service are maintained under existing and buildout conditions.

Policy CF-1c: Require development, infrastructure, and long-term planning projects to be consistent with all applicable County and service provider infrastructure master plans.

Policy CF-1d: Require development projects to offset or mitigate impacts to community services and facilities to ensure that service levels for existing users are not impaired by new development.

Policy CF-1e: Require development projects to install off-site infrastructure or pay in-lieu fees when appropriate.

Policy CF-1f: Require new utilities in the Plan area to be installed underground.

Policy CF-1g: Design infrastructure improvements and facilities to enhance groundwater recharge, reduce and address water and sewer demand, and reduce susceptibility to hazards, including flooding and fire.

Policy CF-1h: Annually identify infrastructure needs that should be prioritized in the Capital Improvement Plan to address existing deficiencies and to accommodate planned development.

Policy CF-1i: Require all future development projects sized beyond existing size and density to obtain written verification of availability of water and wastewater capacity.

5-8	Overview 5-1	Water 5-2	Sewer 5-4	Storm Drains 5-5	Dry Utilities 5-6	Emergency Services 5-7	Goals and Policies 5-8
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I. OVERVIEW

This chapter of the Springs Specific Plan includes an implementation program intended to facilitate the public improvements needed to fully implement the Specific Plan. The implementation program consists of three major components:

- A. **Action Plan.** The Action Plan lists key public improvements required by the Specific Plan and summarizes the fundamental steps needed to facilitate each improvement. The action plan also includes rough timeframes, cost estimates, potential funding sources, and the agency primarily responsible for the improvements listed.
- B. **Funding.** The Funding and Financing section discusses potential sources of funding to implement the Action Plan.
- C. **Incentives.** The Incentives section identifies various tools that can be used to incentivize private investment in public improvements.

II. PLAN ADMINISTRATION

The Sonoma County Permit and Resources Management Department is responsible for the administration, implementation, and enforcement of this Plan.

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III. ACTION PLAN

The Action Plan (Table 10) lists the specific public improvements that are needed to fully implement the Specific Plan. The Action Plan includes a brief summary of the primary actions required to facilitate the improvements listed, as well as the agencies and departments responsible for carrying them out. Rough timeframes, cost estimates, and potential funding sources are also included. Table 9 provides a legend for the timeframe and cost descriptions used in the Action Plan.

This Action Plan will be used by the County throughout the life of the Springs Specific Plan, and should be periodically reviewed and updated to reflect conditions as they change over time.

Table 9: Action Plan Legend

Timeframe	
Near	1 to 2 years
Short	2 to 4 years
Medium	4 to 8 years
Long	Over 8 years
Ongoing	Throughout the Life of the Plan
Cost	
\$	< \$50,000
\$\$	\$50,000 - \$250,000
\$\$\$	\$250,001 - \$750,000
\$\$\$\$	Over \$750,000
Responsibility	
Permit Sonoma	Sonoma County Permit & Resource Management Depart
Public Works	Sonoma County Transportation and Public Works Depart
Water District	Valley of the Moon Water District
Sewer District	Sonoma Valley Sanitation District
Sonoma Transit	Sonoma County Transit

6-2	Overview 6-1	Plan Administration 6-1	Action Plan 6-2	Funding 6-8	Incentives 6-15
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Table 10: Action Plan

Action	Timeframe	Cost ¹	Responsibility	Potential Funding Source
Community Amenities				
1. Community Plaza Design (Phase 1) and develop (Phases 2 and 3) a central plaza to provide a gathering and event space for the community.	Phase 1: Near Phase 2: Short Phase 3: Med/Long	Phase 1: \$\$ Phase 2: \$\$\$ Phase 3: \$\$\$\$	Permit Sonoma Public Works	General Fund; Support the Springs Campaign; Development Impact and In-Lieu Fees; Block Grant Funds
2. Gateway Improvements Design (Phase 1) and install (Phase 2) new gateway monument-style signs and landscaping to create a sense of arrival and community.	Phase 1: Short Phase 2: Med	Phase 1: \$ Phase 2: \$\$	Permit Sonoma Public Works	General Fund; Support the Springs Campaign
3. Street Furniture Install new street furniture in accordance with the Design Guidelines.	Short	\$\$	Permit Sonoma Public Works	General Fund; Support the Springs Campaign; One Bay Area Grant
Transportation				
4. Bicycle and Pedestrian Facilities As part of each Capital Improvement Program cycle, construct bicycle and pedestrian improvements identified in Tables 3 and 4	Short/Med	\$\$ - \$\$\$\$	Public Works	General Fund, Development Impact and In-Lieu Fees; Funding District; Measure M Funds; Block Grant Funds; One Bay Area Grant; State and Federal Programs.
5. Fill Sidewalk Gaps As part of each Capital Improvement Program cycle, identify sidewalk gap priorities and construct at least one segment annually.	Ongoing	\$\$ to \$\$\$/year	Public Works	General Fund, Development Impact and In-Lieu Fees; Developer Contributions; Funding District; Block Grant Funds; State and Federal Programs

Action	Timeframe	Cost ¹	Responsibility	Potential Funding Source
<p>6. Transit Improvements Coordinate with Sonoma Transit to increase awareness of the Route 32 shuttle, provide a bench, shade, and adequate signage at each bus stop, and increase frequency of service.</p>	Short/Med	\$\$\$	Permit Sonoma Sonoma Transit	General Fund, Development Impact and In-Lieu Fees; Measure M Funds; State and Federal Programs
<p>7. Highway 12 Corridor Improvements Prepare plans (Phase 1) for new cross-walks, restriping of cross-walks and travel lanes, green bike lanes, and modification of medians and curb lines, as described in Chapter 3. Install improvements in phases, prioritizing safety features.</p>	Phase 1: Short Phase 2: Med Phase 3: Long	Phase 1: \$\$ Phase 2: \$\$\$ Phase 3: \$\$\$ Phase 4: \$\$\$\$	Public Works	General Fund, Development Impact and In-Lieu Fees; Developer Contributions; Funding District(s); Measure M Funds; One Bay Area Grant; State and Federal Programs
<p>8. West Thomson, Lichtenberg, and Hawthorne Ave Improvements Prepare plans (Phase 1) and implement (Phase 2) the cross-section changes to West Thomson, Lichtenberg, and Hawthorne Ave to provide additional on-street parking.</p>	Phase 1: Short Phase 2: Med	Phase 1: \$ Phase 2: \$\$	Public Works	General Fund, Development Impact and In-Lieu Fees; Developer Contributions; Funding District
<p>9. Highway 12/Calle del Monte Traffic Signal: Monitor the need for signalization through review of traffic impact analyses prepared for projects in the vicinity. Add this improvement to the Sonoma Valley Development Impact fee program, Capital Improvement program, or appropriate mechanism to ensure future funding. Coordinate approval and installation of the signal with Caltrans.</p>	Medium/Long	\$\$\$	Public Works	General Fund, Development Impact and In-Lieu Fees; Developer Contributions
<p>10. Highway 12/Donald Street Traffic Signal: Monitor the need for signalization through review of traffic impact analyses prepared for projects in the vicinity. Add this improvement to the Sonoma Valley Development Impact fee program, Capital Improvement program, or appropriate mechanism to ensure future funding. Coordinate approval and installation of the signal with Caltrans.</p>	Medium/Long	\$\$\$	Public Works	General Fund, Development Impact and In-Lieu Fees; Developer Contributions

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Action	Timeframe	Cost ¹	Responsibility	Potential Funding Source
<u>11. Donald Street/Robinson Road Controls:</u> Monitor the need for intersection controls through review of traffic impact analyses prepared for projects in the vicinity. Add this improvement to the Sonoma Valley Development Impact fee program, Capital Improvement program, or appropriate mechanism to ensure future funding.	Medium/Long	\$/\$\$	Public Works	General Fund, Development Impact and In-Lieu Fees; Developer Contributions
<u>12. Coordinate Timing of Signals along Highway 12:</u> Add this improvement to the Sonoma Valley Development Impact fee program, Capital Improvement program, or appropriate mechanism to ensure future funding. Coordinate approval and installation of the signal with Caltrans.	Short	\$\$	Public Works	General Fund, Development Impact and In-Lieu Fees; Developer Contributions
Parking				
<u>9. Parking Lot – Northern Plan Area</u> Acquire land, prepare plans, and construct parking lot(s) for 35 vehicles in the northern portion of the Plan area.	Phase 1: Short Phase 2: Med	Phase 1: \$\$ Phase 2: \$\$\$\$	Public Works	General Fund, Development Impact and In-Lieu Fees; Funding District
<u>10. Parking Lot – Boyes Blvd.</u> Acquire land (Phase 1), prepare plans (Phase 2), and construct parking lot(s) (Phase 3) for 150 – 200 vehicles within walking distance of the Hwy 12/Boyes Blvd intersection.	Phase 1: Short Phase 2: Short/Med	Phase 1: \$\$\$\$ Phase 2: \$\$/\$\$\$ Phase 3: \$\$\$\$	Public Works	General Fund, Development Impact and In-Lieu Fees; Funding District(s); Revenue Bonds
Service Infrastructure				
<u>11. Sewer Infrastructure Improvements</u> Prepare plans to repair, retrofit, and expand sewer infrastructure addressing baseline defects and capacity deficiencies and to accommodate anticipated development (Phase 1). Construct improvements (Phase 2).	Phase 1: Short Phase 2: Med/Long	Phase 1: \$\$/\$\$\$ Phase 2: \$\$\$/\$\$\$\$	Public Works Sewer District	General Fund, Development Impact and In-Lieu Fees; User Fees and Rates; Sewer Grant Programs

Action	Timeframe	Cost ¹	Responsibility	Potential Funding Source
<p><u>12. Water Infrastructure Improvements</u> Prepare plans to repair, retrofit, and expand water infrastructure addressing baseline defects and capacity deficiencies and to accommodate anticipated development (Phase 1). Construct improvements (Phase 2).</p>	Phase 1: Short Phase 2: Med/Long	Phase 1: \$\$/\$\$\$ Phase 2: \$\$\$/\$\$\$\$	Public Works Water District	General Fund, Development Impact and In-Lieu Fees; User Fees and Rates; Water Grant Programs
<p><u>13. Storm Drain Infrastructure Improvements</u> Prepare plans to repair, retrofit, and expand storm drain infrastructure addressing baseline defects and capacity deficiencies and to accommodate anticipated development (Phase 1). Construct improvements (Phase 2).</p>	Phase 1: Short Phase 2: Med/Long	Phase 1: \$\$/\$\$\$ Phase 2: \$\$\$/\$\$\$\$	Public Works	General Fund, Development Impact and In-Lieu Fees; User Fees and Rates; Storm Water Grant Programs
Funding				
<p><u>14. Reimbursement District</u> Establish a Reimbursement District or other mechanism that allows for a private developer to fund the construction of sewer, water, storm drains, streets, streetscape, or public utilities infrastructure.</p>	Short	\$	Permit Sonoma	General Fund; Developer Funds
<p><u>15. Grant Funding Applications</u> Regularly search for potential grant funding sources to fund improvements listed in this Action Plan. Submit applications for grants that have a reasonable likelihood of success.</p>	Ongoing	\$	Permit Sonoma Public Works	General Fund
<p><u>16. Nexus Study for Specific Plan Development Impact Fees</u> Undertake a nexus study to quantify the potential infrastructure needs and related costs of future development within the Plan area. Utilize study as a basis for updating the County’s existing Development Impact Fee schedule to include additional fees unique to the Plan area (e.g. plaza, parking, streetscape, gateway, and infrastructure improvements). The cost of the study can ultimately be recovered from impact fees.</p>	Near	\$\$	Public Works	General Fund, Development Impact and In-Lieu Fees

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Action	Timeframe	Cost ¹	Responsibility	Potential Funding Source
Procedures				
<u>17. Development Review</u> Review all development proposals and building permit applicants in the Springs and ensure consistency with the goals, policies, and design guidelines established by this Plan.	Ongoing	\$	Permit Sonoma	Permit Fees
<u>18. Public Projects Review</u> Review all public projects proposed in the Springs to ensure consistency with the goals, policies, and design guidelines established by this Plan.	Ongoing	\$	Permit Sonoma Public Works	General Fund; Development Impact Fees
<u>19. Informational Brochure</u> Create an informational brochure that explains the permit process for projects in the Springs (General Plan, Specific Plan, and zoning requirements) and the development incentives (see Table 11) available in the Springs.	Near	\$	Permit Sonoma	General Fund
<u>20. Priority Sites</u> Identify priority sites for development and reuse. Perform pre-development and preliminary design work to encourage investment and development of the sites.	Short	\$\$	Permit Sonoma	General Fund

1: Costs do not include operations and maintenance.

IV. FUNDING AND FINANCING

Funding Sources

This section provides brief descriptions of the potential funding sources and financing tools that may be used to facilitate the capital improvements identified in the Specific Plan.

Local

General Fund

General Fund revenues include property tax, sales tax, transient occupancy tax, and other revenues that are primarily used to pay for ongoing municipal services and operations. There are no restrictions on the types of capital projects that can be funded with General Fund revenues.

Existing Connection Fees

Connection fees are one-time fees charged for connecting to the public water and sewer system in order to reimburse the County or service district for the cost of providing these facilities. Connection Fee revenues can only be used to pay for improvements to the type of system for which the fee is charged. For example, water fees can only be used to pay for improvements to water mains and other water infrastructure.

User Fees

User fees are charged for the on-going use of public infrastructure, such as parks, water or wastewater systems, or public parking facility. User fees are typically set to cover a system’s operating and capital expenses each year, which can include debt service for improvements to the system. User fees charged for parking in publicly owned parking spaces could also be used to pay for the construction, operation, and maintenance of a public parking facility. Revenues collected from these sources are typically collected in an enterprise fund, which is then used to fund eligible projects.

Development Impact Fees and In-Lieu Fees

Development impact fees are one-time charges imposed under the Mitigation Fee Act. These fees are charged to mitigate impacts resulting from the development activity, and cannot be used to fund improvements to remedy existing deficiencies. This condition means that for improvements that benefit existing as well as new development, impact fees can only pay for the portion of the improvement that benefits the new uses. Impact fees are adopted based on findings

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of a reasonable relationship between the development paying the fee, the size of the fee, and the use of fee revenues. Impact fees can be a funding source for transportation, utility infrastructure, streetscape, parking, recreation, and other improvements. The County currently charges a variety of impact fees, including park mitigation, traffic mitigation, and affordable housing fees. New development in the plan area will be charged impact fees, and the revenue generated must be used for capital improvements needed to serve the new development.

Similar to impact fees, in-lieu fees allow a developer to pay a fee to satisfy a requirement that would otherwise entail providing infrastructure, an amenity, or mitigation measure on-site, such as parking or affordable housing.

Developer Contributions

Developer contributions are payments made in addition to normal impact fees as part of the development approval process to fund public improvements. Developers may also contribute by financing and constructing certain public improvements required for their project. Developer contributions most often apply to larger developments with significant associated impacts. Structured negotiations between the County and a developer may be conducted to determine the extent of improvements to be provided in exchange for development rights. The degree to which a new project would contribute to the provision of infrastructure depends on a number of factors, including the cost of the improvements, the scale of the development project, anticipated revenues that will be generated by development, construction costs, and on-site parking requirements.

Statewide Community Infrastructure Program

The Statewide Community Infrastructure Program (SCIP) is a program of the California Statewide Communities Development Authority, which is a joint powers authority sponsored by the League of California Cities and the California State Association of Counties. Under SCIP, the authority can form assessment districts, levy assessments, and issue tax-exempt revenue bonds. Sonoma County currently participates in SCIP. The program allows developers and property owners to form an assessment district through SCIP to pay for impact fees and infrastructure improvements. Assessments are then paid to SCIP over a set timeframe. SCIP provides other benefits including forming assessment districts, levying the assessments, and issuing the bonds.

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Funding Districts

There are a variety of district types that can be established to provide funding for a range of infrastructure and public improvements. These are described below.

Assessment Districts

In an assessment district, property owners agree to levy an additional tax or assessment on their property in order to fund specific improvements or services. The majority of affected property owners must vote in favor of district formation. California law defines a number of assessment districts, including Lighting and Landscaping Districts, Parking Districts, Property and Business Improvement Districts. Most of these district may issue tax-exempt bonds.

Under Proposition 218, a constitutional amendment passed by California voters in 1996, the amount that each property owner pays must be directly proportional to the “special benefit” the property will receive from the proposed improvement. As a result, assessment districts are typically used to fund primarily local-serving infrastructure such as landscaping, lighting, street and sidewalk improvements.

Property-Based Improvement District or Business Improvement District

Business Improvement Districts and Property-Based Improvement Districts are types of assessment districts in which business or property owners vote to assess themselves a fee to fund programs and projects to enhance the business community. Typically, these improvement districts provide funding for marketing campaigns, security, streetscape improvements, and special events. By pooling private resources, business owners in improvement districts collectively pay for activities which they could not afford on an individual basis.

Enhanced Infrastructure Financing Districts

An Enhanced Infrastructure Financing District (Enhanced District) can be established by the County to capture a portion of the growth in property tax revenues resulting from new development and increasing property values to fund public facilities, infrastructure, and housing. The restrictions associated with the establishment of Enhanced Districts are lower than for traditional Infrastructure financing districts. Enhanced Districts have not yet been widely utilized, but are now more likely to be established than traditional districts. The Enhanced District financing plan must be approved by the taxing entities affected by the district.

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Community Facilities Districts

The Mello-Roos Community Facilities Act of 1982 was created to allow the establishment of a Community Facilities District (CFD) for the financing of public improvements and services. A Community Facilities District levies additional property taxes on land located inside the district, creating a dependable revenue stream that can be used to pay for improvements. Tax revenues may also be used to issue bonds to fund higher cost projects. A variety of improvements may be financed by a Community Facilities District including new streets, sewer systems, schools, parks, libraries, museums and other cultural facilities, etc.

A Community Facilities District is formed when a two-thirds majority of the voters residing within the proposed district boundary agree to impose a new tax on property in order to fund the project. If there are fewer than 12 residents living within the proposed boundaries, the vote is instead conducted of current landowners. Community Facilities Districts are most commonly formed in undeveloped areas and involves a single property owner or a small number of property owners who intend to develop the property or subdivide the land. The Mello-Roos Community Facilities District Act allows costs to be proportionally passed on to the future owners.

The special tax rate is not based directly on the value of the property. Instead, the special tax is based on property characteristics, such as the use of the property, building square footage, and lot size.

Reimbursement District

A Reimbursement District may be formed when a developer funds the construction of infrastructure that has the potential to benefit subsequent development projects. Future developers who benefit from the infrastructure during the life of the reimbursement district (typically 10-20 years) are assessed a fee to cover their fair share of the improvements. The process to establish a Reimbursement District includes preparation of a reimbursement agreement and report detailing improvement costs, method of distributing the costs, properties involved, and other information. The Board of Supervisors would then convene a public hearing where owners of property in the district are given the opportunity to provide input. Once approved by the Board, a resolution is recorded against properties in the district. Reimbursements are collected when properties in the district develop or make use of the improvements.

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Revenue Bonds

Public activities that are revenue generating, and create sufficient cash flow to cover operating costs and debt service can potentially issue municipal bonds to cover the cost of capital improvements. A common example of this is a revenue bond for construction of a parking garage that has paid parking.

Measure M Funds

Funds from the Traffic Relief Act for Sonoma County (Measure M) are distributed by the Sonoma County Transportation Authority to finance multi-modal transportation improvements throughout the county. Approval of Measure M in 2004 resulted in a ¼ cent increase in sales tax over 20-years and is anticipated to raise between \$17 and \$35 million a year through FY 2024–25. The measure provides funding for local street projects, widening Highway 101, improving local bus service, and bicycle and pedestrian improvements.

Community Development Block Grant Entitlement Funds

Sonoma County is an entitlement jurisdiction for the federal Community Development Block Grant Program, meaning that the County receives an annual allocation of Block Grant funds. The Block Grant funds can be used for a wide variety of housing and community development projects, including public facilities and infrastructure. The County must demonstrate that Block Grant funds will benefit lower income persons.

One Bay Area Grant Program

The One Bay Area Grant Program provides grants for local street and road maintenance, bicycle and pedestrian improvements, and streetscape enhancements. The Sonoma County Transportation Authority administers the capital grant program in Sonoma County. In late 2015, the Metropolitan Transportation Commission adopted a funding and policy framework for the second round of One Bay Area Grants, known as OBAG 2. This round of funding is projected to total about \$916 million to fund projects from 2017–18 through 2021–22 in the Bay Area. Approximately \$28 million was programmed in Sonoma County, including both the incorporated and unincorporated areas. Future rounds of One Bay Area Grants are anticipated and it is likely that the Springs Specific Plan pedestrian, bicycle, and transit components may be eligible for funds.

Support the Springs

A “Support the Springs” campaign, sponsored by local businesses or residents, would be an innovative way to provide supplemental funding for community improvement projects. The campaign could be used to fund streetscape

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improvements such as street furniture, landscaping, and lighting. A small plaque might be provided to recognize the name of the business or person sponsoring the improvement or fixture. A mosaic or other feature might also be displayed at the Plaza to recognize sponsors and supporters that either provided general funding toward the plaza or funded a specific feature, such as a fountain or bench.

State and Federal

State Transportation Improvement Program

The State Transportation Improvement Program is a multi-year capital improvement program that can be used to fund a wide variety of transportation capital projects, including improvements to State highways and local roads, public transit (including buses), intercity rail, pedestrian and bicycle facilities, and inter-modal facilities. Fund programming generally occurs every two years. The County would work through the Sonoma County Transportation Authority to include projects in the Regional Transportation Improvement Plans to be nominated for funding.

Active Transportation Program

The Active Transportation Program is administered by the Caltrans Division of Local Assistance and consolidates existing federal and state transportation funding programs, including the Transportation Alternatives Program, Bicycle Transportation Account, and State Safe Routes to School, into a single program. The purpose of ATP is to encourage increased use of active modes of transportation. The current cycle of funding includes approximately \$240 million.

California Office of Traffic Safety Grants

The California Office of Traffic Safety awards federal funding on a competitive basis to cities and counties for programs that help them enforce traffic laws, educate the public in traffic safety, and provide varied and effective means of reducing fatalities, injuries, and economic losses from collisions. Evaluation criteria for grants include potential traffic safety impact, collision statistics ranking, seriousness of identified problems, and performance on previous grants.

Water, Sewer, and Stormwater Grant Programs

State and federal agencies periodically make competitive grant funding available for water and sewer programs. These programs change over time depending on funding availability. Various state bond measures have provided grant funding for programs administered by the California Department of Water Resources and the State Water Resources Control

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Board, such as the Integrated Regional Water Management Program Implementation Grant for water supply and water quality, wastewater and recycled water, flood protection and stormwater management, and watershed management projects; and the Stormwater Flood Management Grant program for projects that manage stormwater runoff to reduce flood damage, improve groundwater supplies, improve water quality, and restore ecosystems.

Other Funding Programs

State and regional agencies periodically offer competitive grants for pedestrian, bicycle, streetscape, road, park, infrastructure and other public improvements. These programs change over time depending on funding availability. Recent examples include the Caltrans Safe Routes to School program; the Transportation Fund for Clean Air program administered jointly by the Bay Area Air Quality Management District and Sonoma County Transportation Authority; the Housing-Related Parks Program and Infill Infrastructure Grant Program administered by the California Department of Housing and Community Development; and the federal Land and Water Conservation Fund and the State Urban Parks and Healthy Communities Program administered by the State Parks Office of Grants and Local Services.

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V. INCENTIVES

Table 11 summarizes potential incentives that may be used to encourage development and improvements consistent with the Specific Plan. The table also includes current developer incentives and loan programs to promote affordable housing and facilitate improvements to existing development.

Table 11: Development Incentives

Incentive	Fee Assistance and Reductions	Funding Source
Reduce Planning Permit Requirements	Planning permit requirements will be reduced for some types of projects such as mixed-use development, work/live projects, and parking lots.	N/A
Streamline Environmental review process	The Program EIR developed for the Specific Plan simplifies the environmental review process for subsequent projects, reducing time and costs for developers. Environmental analysis would be limited to the project-specific effects that were not fully examined by the EIR.	N/A
Development Impact Fees	Create a special district that allows developers to pay impact fees over a specified period of time (e.g., 20 or 30 years).	Statewide Community Infrastructure Program; Assessment District
Expedited Plan Check or Planning Application Processing	Expedite processing for projects that provide affordable housing, significant streetscape improvements or public open space consistent with this Plan.	General Fund

Affordable Housing Density Bonus and Incentives (Current Program)	The County currently provides density bonuses and incentives to encourage development of affordable housing. The County’s program includes State Density Bonus and supplemental density bonus provisions. See Sections 26-89-050, 26-89-060 of the County Code.	General Fund
Program	Grants and Loans	Funding Source
Commercial Property Rehabilitation Loan (Current Program)	The Community Development Commission administers a Commercial Property Rehabilitation Loan Program to provide below market rate loans to eligible business and commercial property owners.	Block Grant Funds; HOME Funds
Housing Rehabilitation Loan (Current Program)	The Community Development Commission administers a Housing Rehabilitation Loan Program to provide financial assistance to low-income home owners to make necessary repairs to their dwellings.	Block Grant Funds
Business Loans (Current Program)	The Economic Development Board administers several loan programs to support entrepreneurs and small businesses.	Public/Private/Non-profit Partnerships

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